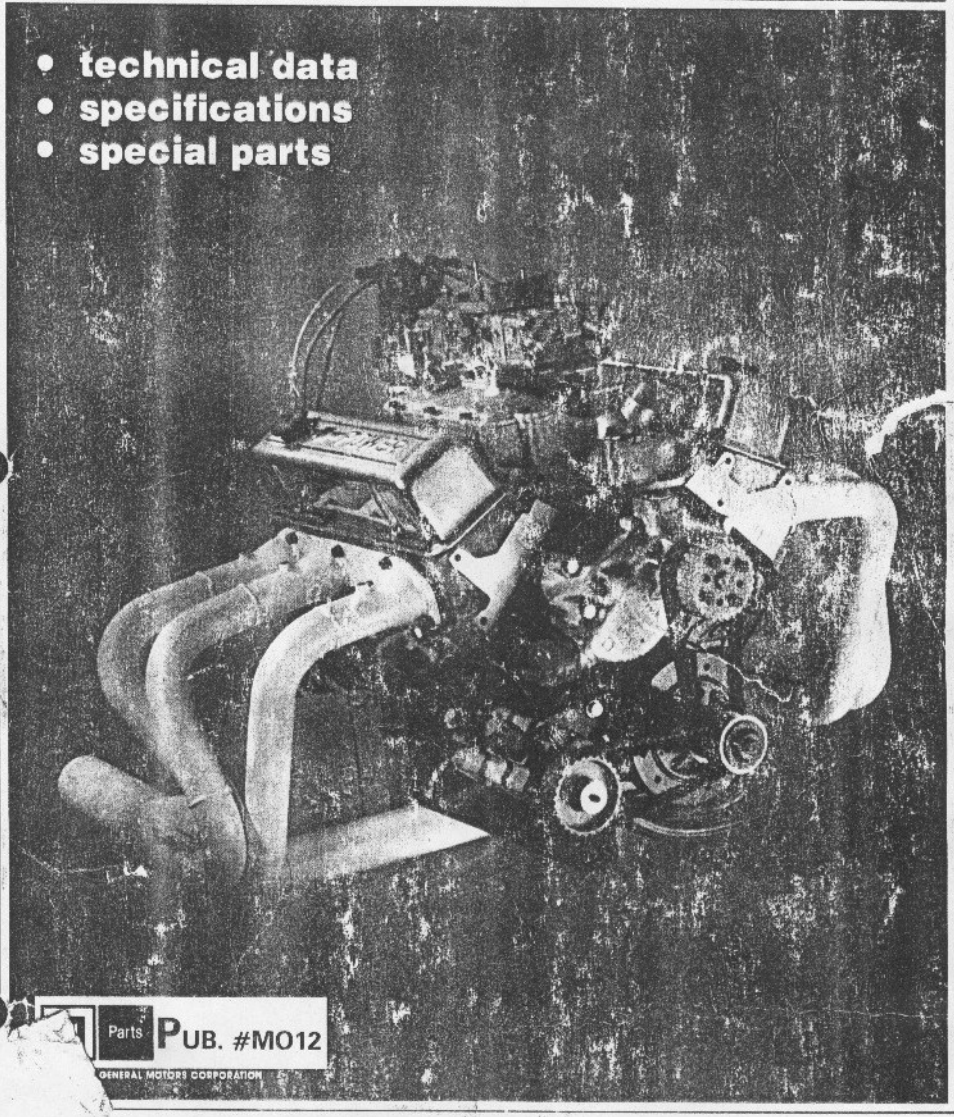
CHEVROLET POWER

5th EDITION

- technical data
- specifications
- special parts



Parts PUB. #MO12

GENERAL MOTORS CORPORATION

CORVETTE CHASSIS

CORVETTE SECTION

Corvette Section

CORVETTE CHASSIS PREPARATION

The following specifications and modifications procedures are intended to assist an individual preparing a Corvette chassis for road course competition such as the I.M.S.A. GT series or S.C.C.A. Trans-Am series.

A thorough knowledge of the current competition rules is essential since the sanctioning organizations rules and interpretation of existing rules frequently change.

These suggested modifications such as frame reinforcements or rewelded seams are only required for off-highway competition activities where abnormal stresses are encountered and for a roll over safety cage installation. It does not imply any inherent structural deficiencies exist in the production components for normal highway usage.

Frame Preparation

The frame should be removed from the vehicle for thorough cleaning (sand blast preferably) and preparation. Use a new 1969 or later Corvette frame or a good solid non-rusted 1969 or later used frame. A new frame is recommended to minimize the possibility of weakening due to any corrosion encountered during service life. The 1969 and later frames incorporate corner braces (Figure 1) between the differential carrier front crossmember and the frame kickup over the axle shafts which provide increased lateral stiffness at the rear suspension attachments.

All the welds should be full length arc welds, since the production frame has skip welding on it. (Figure 2)

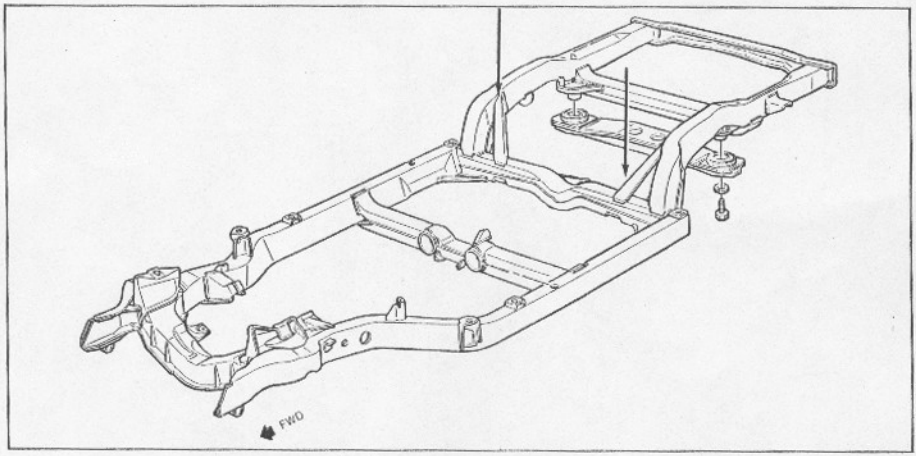
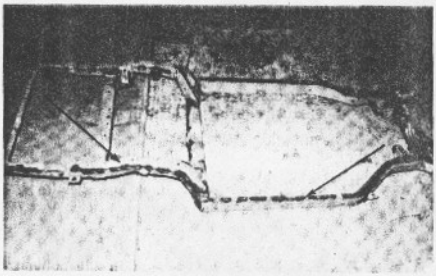


Figure 1 — Desired corner braces providing increased lateral stiffness.



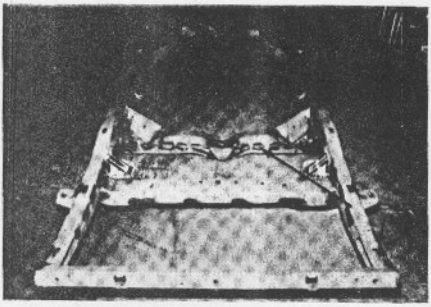


Figure 2 — Frame showing the full length arc weld.

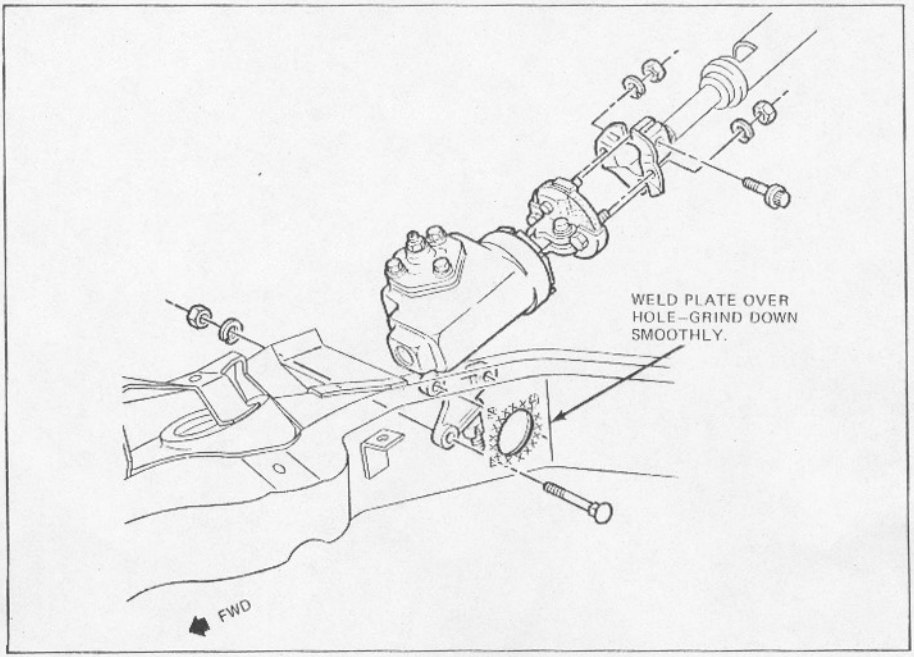


Figure 3 — Required frame reinforcement near the steering gear assembly.

All Manufacturing holes or clearance holes that are not to be used should be plated over and welded. Of particular importance is the hole (Figure 3) near the steering gear in the left side rail which should be filled with a plate, welded and then ground smooth to prevent tire interference.

An automatic transmission frame is recommended because it has a removable crossmember (Figure 4). If an automatic transmission frame is used, the clutch cordon shaft bracket must be welded to the frame, and the manual transmission mounting bracket adapted to the crossmember.

Weld in the roll cage which must conform to sanctioning organization safety requirements, as shown in Figure 5. The intent of the roll cage is not only to provide protection for the driver, but to increase the total stiffness, both bending and torsional, of the frame and body assembly.

Therefore, running struts to the front and rear suspension points attachments is mandatory. It is advisable to put a front suspension tie bar, either welded in or bolted in, between the front upper control arm towers to minimize compliance of the upper control arm attachment points.

The torsional rigidity of the flange is also greatly increased by replacing the production rubber engine mounts with a solid engine mounting plate at the front of the block & solid brackets from the clutch housing to the frame.

Gusseting (Figure 6) should be added to the upper control arm towers and the lower control arm attachment channels in the form of "fish plates".

The entire frame and suspension should be painted with a light colored chemical resistant paint such as epoxy after all welding is completed to prevent corrosion and to facilitate detection of frame cracks during subsequent inspections.

Clearance (Figure 7, page 4-4) should be cut in the front coil spring pocket in the frame for the shock absorbers to be used in the car.

The clearance should be checked in both full rebound and full jounce to prevent possible bending of the shock absorber shaft during suspension travel.

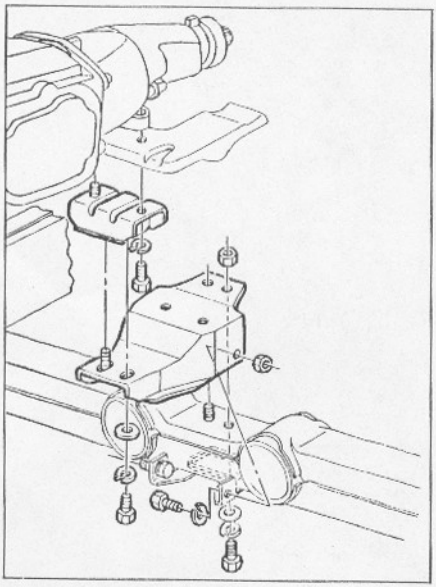


Figure 4 — Installation of manual transmission mounting bracket.

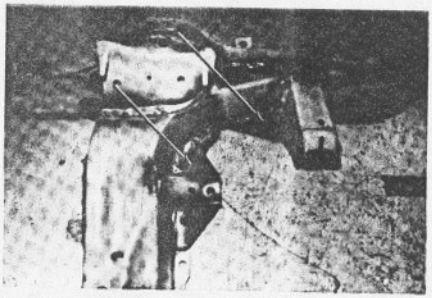


Figure 6 — Weld gussets to crossmembers and lower control arm bracket.

The rear suspension differential carrier mounting crossmember should have the rubber mounts removed and replaced with steel plates. (Figure 8, page 4-4) There is some freedom to adjust the height of these plates to mount the differential carrier further up into the frame. If this is done, the corresponding adjustment should be made in the front differential case attachment.

The differential mount crossmember may also be replaced by a fabricated tube to raise the differential approximately 1 inch higher. The rear frame rails must also be trimmed to raise the bump travel & the shock absorber brackets are relocated upward 1 inch.

The frame should be boxed in at the front frame rails forward of the front suspension crossmember over the stabilizer bar attaching points (Figure 9, page 4-5) to provide adequate stiffness for stabilizer bar loads to be imparted to the chassis.

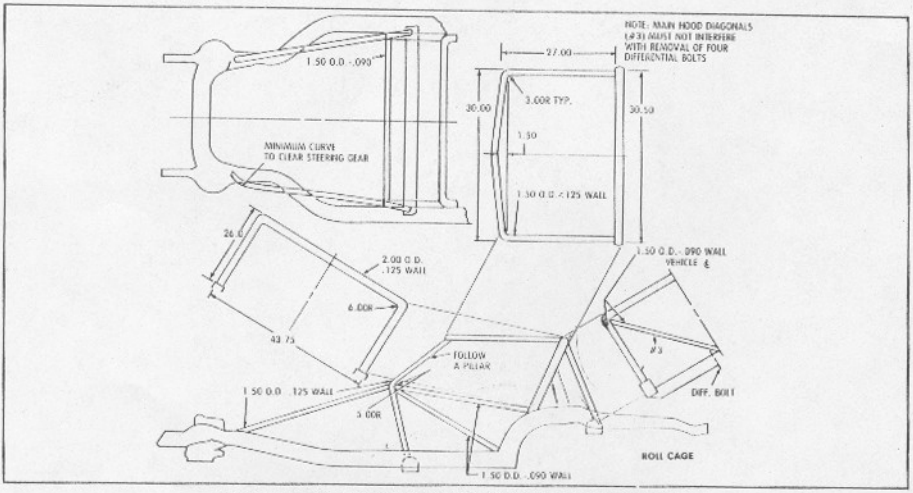


Figure 5 — Typical roll cage welded to the frame assembly.

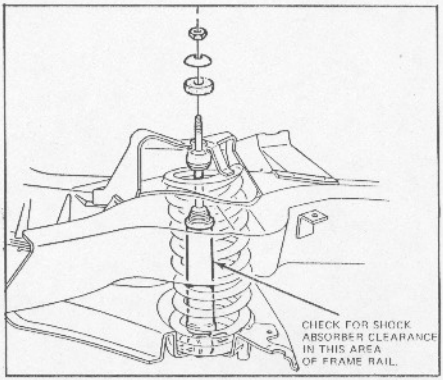


Figure 7 — Area of frame rail requiring shock absorber clearance.

If the rules permit, tire clearance should be provided in the frame rails and the area must be cleaned up with a grinder to prevent sharp surfaces which could possibly cut a tire during cornering. Likewise; at the front the area just aft of the front suspension crossmember should be smoothed up to prevent tire interference.

Front Suspension

Use production upper and lower control arms. Weld stabilizer bar attachments to lower control arm to reinforce the attachment. (Figure 10)

Replace the rubber control arm bushings with solid bushings either by fabricating replacement bushings from bronze or high density plastic, using commercially available solid bushing kit, or using rod end joints welded onto the arms.

Provide variable caster-camber adjustment at the upper control arm by means of threaded attachments. If production type of attachments are used, then replace shims with

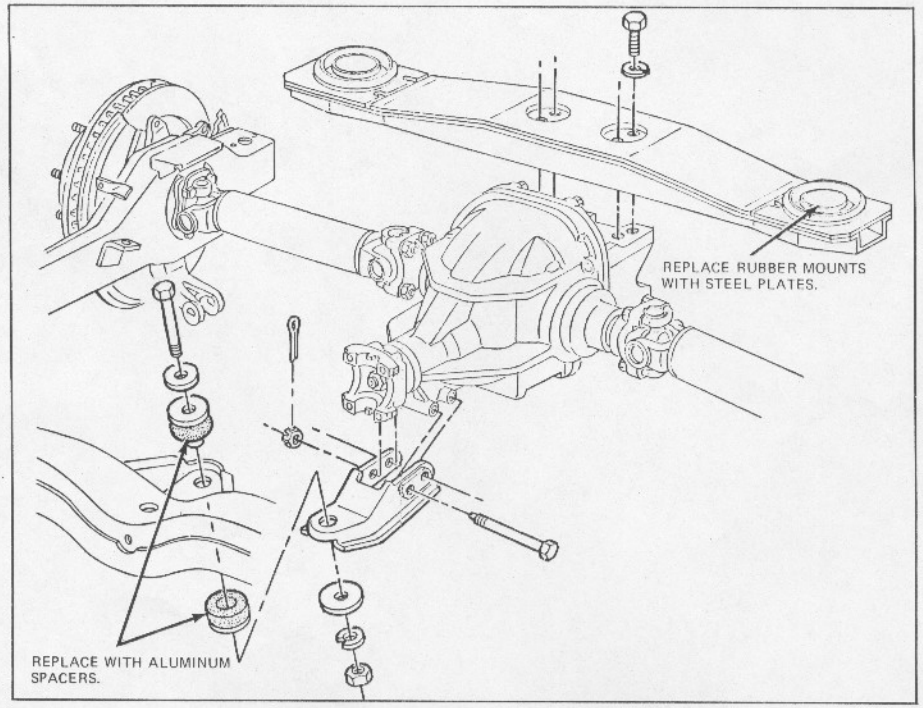


Figure 8 — Replacement of rubber parts with solid plates and bushings.

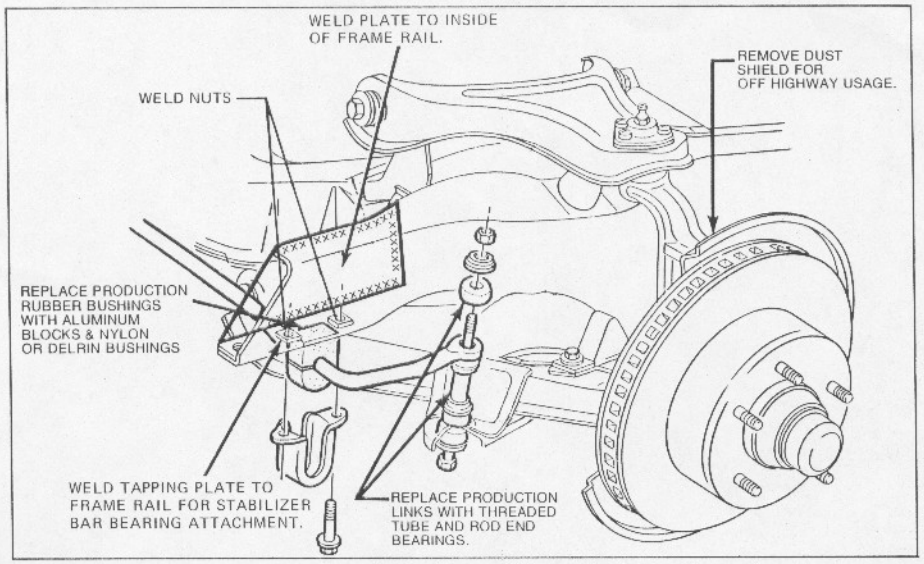


Figure 9 — Weld tapping plate to inside of frame rail for stabilizer bar bearing attachments.

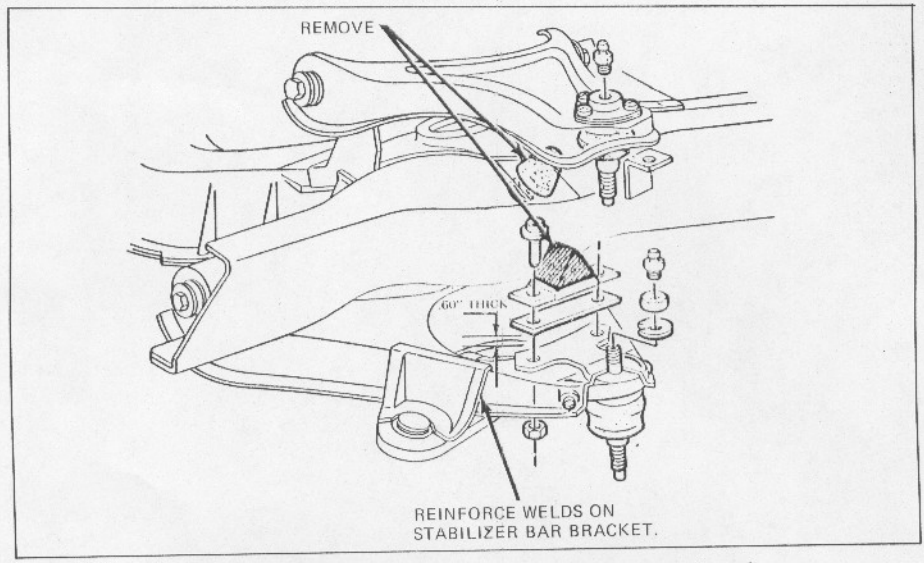


Figure 10 — Reinforce welds on stabilizer bar bracket and remove the upper /lower bumpers.

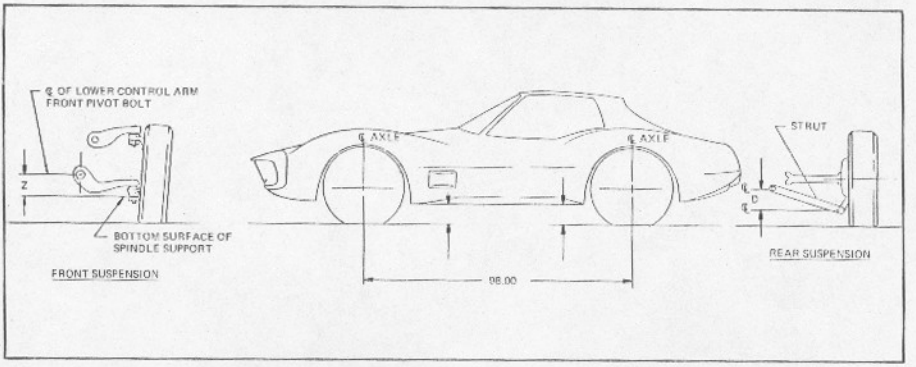


Figure 11 — Proper front and rear vehicle trim heights.

washers to prevent loss of shims if upper control arm shaft attaching bolts are loose.

Use 1969 or later production Corvette front spindle which incorporates the larger diameter wheel bearings. Adjust wheel bearing end play to .001 max. and use production high temperature type wheel bearing grease. Such as Sunoco Sunaplex 802.

Trim rubber jounce bumper to metal-to-metal *(height .60 in.)* see Figure 10, page 4-5 or remove entirely if shock absorber and springs allow suspension travel to the point of control arm bottoming on the frame. Rebound control can be limited by means of shock absorber travel restriction or the application of an external stop in the form of metal-to-metal stop at upper control arm to frame contact point or a strap arrangement to lower control arm. Rebound should be limited to prevent the spring from unseating during its rebound travel. Spring usage should be F-41 front spring for applications on all tracks other than highly banked tracks. On high bank tracks, such as Daytona and Talladega, the "Daytona" front spring package should be used. Spring height should be adjusted by either bumping or bulldozing the spring to achieve a front "Z" height of 1" to 1-1/4" with the vehicle full of fuel and driver aboard. (Figure 11) Bulldozing a coil spring is a technique of lowering the design height by fully compressing the spring to a closed coil condition while lightly tapping the coils with a brass hammer. Extreme caution should be exercised during this procedure since the compressed spring is under considerable force. This will provide adequate suspension travel while maintaining the lower front profile for aerodynamic use. Check for bottoming of oil pan and crossmember at full jounce conditions.

Stabilizer bar application can be production bars or bars similar to production manufactured by the individual. In all cases, they should be mounted in a solid bushing material such as aluminum, nylon, or delrin. The links at the end of the stabilizer bar connecting to the lower control arm should be replaced with rod end joints and a treaded shaft to provide noncompliant links and adjustable lengths.

It is desirable to check the "bump steer" of the front suspension to obtain zero toe change during the jounce portion of wheel travel. A production steering linkage may be adjusted by lowering the outer tie rod point approximately 3/4" which will improve the bump steer curve. An area to watch is possible tire interference between the front tire and the steering arm and/or tie rod end.

When setting the vehicle up after build, the front wheel weight distribution should be as near equal as possible without the stabilizer bar connected. Once the weight has been equalized with the springs installed, the stabilizer bar should be attached and the links should be adjusted so there is zero preload of the front suspension due to stabilizer bar torsion.

Rear Suspension

The differential carrier rear mounting crossbar rework was discussed in the frame preparation section. The rubber differential mounting insulators should be replaced by solid steel plates. The pinion nose front bushing should also be replaced by a solid metal bushing, preferably an aluminum block. Care should be taken to maintain the correct differential case attitude. If the plates have been moved up in the crossmember, this should be duplicated by adjusting the height of the aluminum block the same amount.

Rear suspension torque arms should be replaced with new units purchased from Service. The torque arms should have the parking brake cable mounting bracket removed and the surface ground smoothly. All seams should be completely welded to maintain total strength. The torque arm forward bushing can be replaced with a spherical joint if desired, although many cars run with the production rubber bushing.

It is desirable to install a threaded toe adjustment mechanism in place of the shim pack at the front bushing mounting point. If a threaded adjuster is not used, the late model shims must be used which provide for a cotter pin to hold the shims in place. If tire clearance is required in the torque arm, it is mandatory the torque arm be fixtured before welding to maintain arm alignment. The torque arm section be maintained in the area of the offset and that all material added be of a thicker gage than production. (Figure 12)

The production bushing in the control arm should be replaced with a spherical bearing. Tire clearance in the sus-

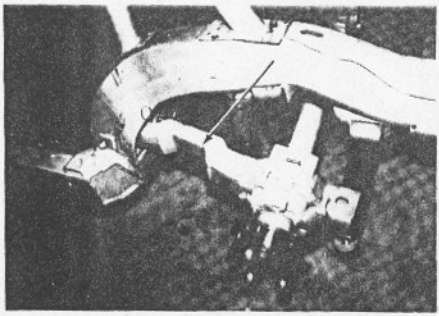


Figure 12 — Reworking rear suspension control arm.

pension control arm should be avoided if possible since undesirable rear wheel steer can result from modified arms.

Use rear spindle part number 3872476 which is a high nickel alloy forging and has proven satisfactory. Use a high pressure grease such as shell alvania EP-2 when building the spindle pack. Set spindle end play to .001" maximum. Delete all the parking brake and dust shield components in the rear spindle assembly as a weight saving measure and to provide additional brake cooling for off highway usage only.

Use the current production 3" diameter axle driveshafts. These units came into production in 1975 and are serviced for all previous Corvettes. Use the cap type attachment instead of the strap type (cap part number 3872909), for attaching the axle to the yoke.

Rework strut rod attachment bracket on the bottom of the axle by welding a plate over the adjustment slot and drilling a single hole to accept rod end joint. Replace the strut rod with a threaded tube using left and right threaded rod end joints. This system provides a noncompliant link as well as positive camber adjustment and control. (Figure 13).

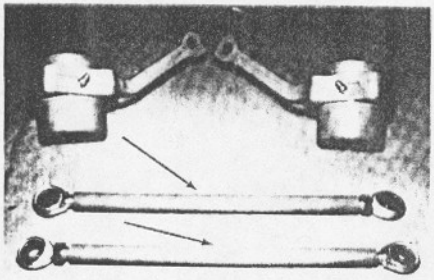


Figure 13 — Rear suspension strut rod replacement.

The lower strut rod attachment should be made with the Corvette heavy duty lower shock mounting pin in place of the production unit for increased shock absorber clearance. Part number of the heavy duty pin is 3829265-66. Use the F-41 rear spring for normal track surfaces. For high bank tracks such as Daytona and Talledaga, use the "Daytona" spring package. When installing the spring, use longer spring hanger bolts made out of high strength steel to provide a means of height adjustment. Set the rear "D" height to 1-1/4" with fuel and driver aboard, as shown on Figure 11.

The use of longer spring bolts allows the adjustment of weight from side to side to equalize corner loads. Trim the rear jounce bumpers to approx. .60" thickness which provides maximum suspension travel while preventing the universal joints from bottoming out during full bump. The production jounce bumpers may be completely removed if the shock absorbers are equipped with rubber bumpers.

If the axle has been moved up in the frame, it may be possible to remove the jounce bumpers entirely. Before this is done, a static check should be made in full jounce position to determine if the universal joints are grounding out. Rebound control should also be checked to make sure universal joints do not ground out in full rebound.

Production rebound control is maintained by shock absorber length. If replacement shock absorbers are used, a check should be made at full rebound to assure universal joints are not grounding out. If the shock cannot do this, a strap or cable should be provided between the control arm and the frame to prevent excessive rebound travel. The production type rear stabilizer bar is satisfactory if the links from the control arm to the stabilizer bar are replaced with rod end joints and threaded shafts to allow adjustment. When installing the bar (Figure 14, page 4-8), make sure the linkage is such that the stabilizer bar will not travel to an "over center" condition in full rebound. It may be necessary to lower the axis of the bar to prevent this.

The F-41 rear spring and the recommended front bar should be used with a 3/4" dia. rear bar. This diameter bar will have to be fabricated to the standard production bar configuration. Care must be taken to provide adequate tire clearance when forming the bar. The "Daytona" spring with recommended front bar can use either a 9/16 production rear bar or possibly a 5/8", depending upon vehicle. It may be necessary to alter these bar diameters slightly to provide the correct balance depending on the individual car and driver preference. In any case, stabilizer mounting bushings to frame should be replaced with a solid material such as delrin, nylon, bronze, aluminum.

It is desirable to replace the shock absorbers with external double adjustable shock absorbers. The early F-41 Corvette heavy duty shocks may also be used. This is a large diameter shock which increases rebound and jounce damping over the standard shock and has greater heat capacity.

CLUTCH AND TRANSMISSION PREPARATION

The following procedures should be followed to insure minimum difficulties and maximum durability from the heavy duty Chevrolet clutch and transmission components.

Clutch

1. The crankshaft pilot bushing should be replaced if the original bushing shows any eccentricity or wear.

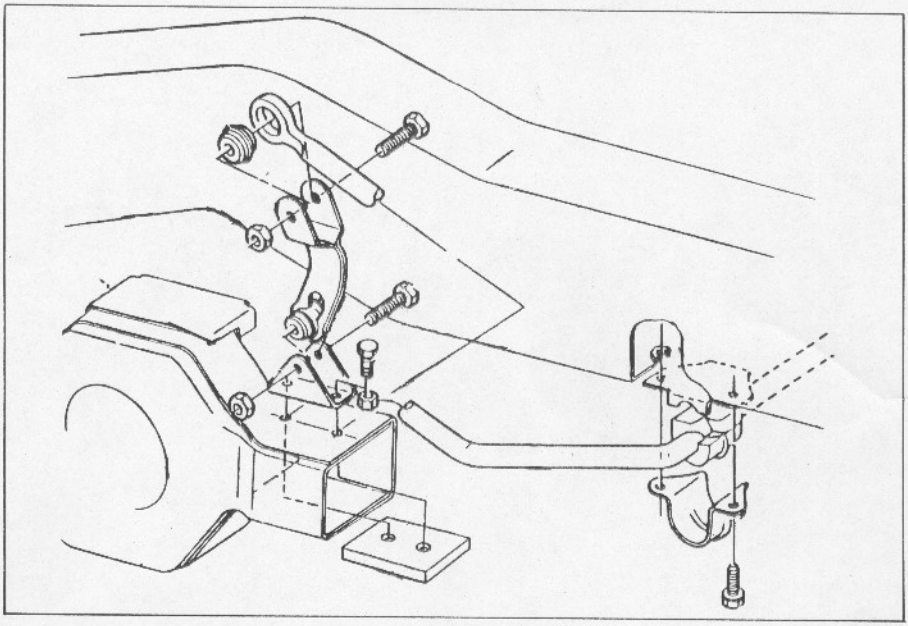


Figure 14 — Rear stabilizer bar installation.

2. The clutch housing transmission mounting face must be parallel to the crankshaft flywheel mounting face or the rear face of the clutch housing should be machined to obtain parallelism.

3. The clutch housing pilot hole for the transmission bearing retainer must be concentric to the crankshaft centerline. Offset dowel pins are available to adjust the concentricity.

The following clutch components have proven to be satisfactory during high performance usage:

1. RPO L-88 nodular cast iron light weight 12-3/4" diameter flywheel (P/N) 3991406).

2. Clutch cover and pressure plate (P/N 6273958) using a nodular iron pressure plate, flat drive strap, higher release load spring and nominal 3200 lb. pressure plate load.

3. Clutch driven plate (P/N 3991428) using a bonded aluminum backed clutch facing, heavy duty splined hub and low height cushion springs.

The clutch driven plate should be checked for any burrs or roughness on the hub splines which might impede movement on the transmission input shaft. It is advisable to "lap" the clutch disc on the clutch gear to remove any burrs. (Figure 15) The clutch driven plate requires a break-in period to remove friction material "fuzz" which will cause the clutch to release incompletely.

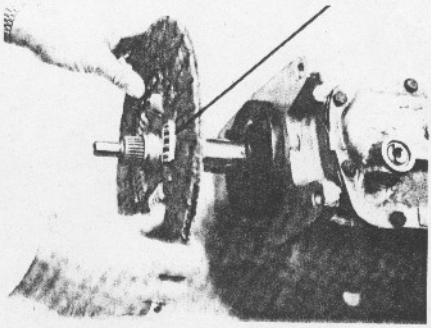


Figure 15 — Lapping the clutch disc on the clutch gear to remove any burrs.

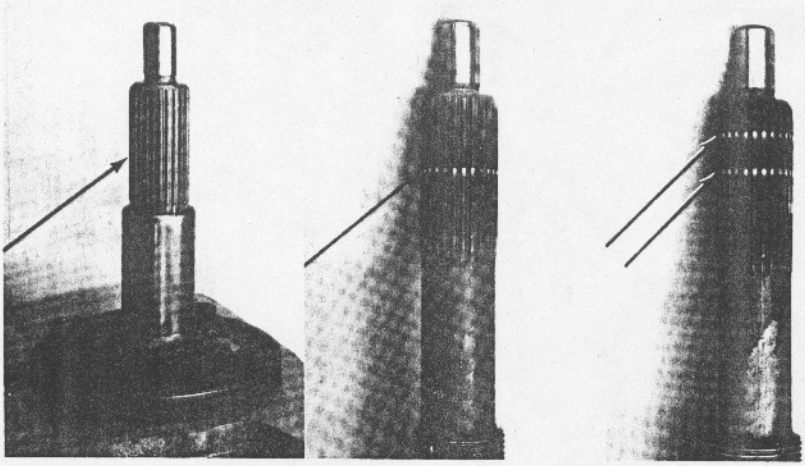


Figure 16 — Illustrated transmission identifying the circumferential grooves on input shaft.

Transmission

The Chevrolet Muncie transmission using a cast aluminum gear case and extension housing was the most durable 4 speed manual transmission offered by Chevrolet for high performance usage. However, the Muncie transmission assembly is no longer produced although all individual components are available from GM Parts Division through Chevrolet dealers. The close ratio Muncie transmission with 2.20:1 first gear ratio, 26 spline input shaft and 32 spline output shaft is recommended for off-the-highway road racing activities. There are three aluminum case Muncie transmissions which may be identified as follows:

1. Heavy Duty Performance (Close Ratio) transmission uses low helix angle gear set, close ratio gears with 2.20:1 first gear, oil drain plug on gear case and no identifying circumferential grooves on input shaft. (Figure 16)

2. Standard Performance Transmission has close ratio gears with 2.20:1 first gear, no oil drain plug on gear case and one circumferential groove on input shaft.

3. Regular Four-Speed Transmission has wide ratio gears with 2.52:1 first gear, no oil drain plug and two grooves on input shaft.

The following special steps should be followed before any transmission is used in competition:

1. Transmission should be thoroughly disassembled for visual inspection as well as magnetic particle and dye penetrant inspection of all components. (Figure 17, page 4-10)

2. Nut or grit blast inside of case to remove casting sand and casting flash.

3. Polish front and rear surfaces of counter gear. (Figure 18, page 4-11)

4. All syncronizer (blocker) rings should be checked for hardness (R_B 75-80 preferred) and checked for roundness either with a dial indicator or with "Prussian Blue" against a cone. (Figure 19, page 4-11)

The blocker ring thread flats should be .002" - .004" wide which can be checked with an optical scale. All burrs should be removed from the blocking ring chamfers. Add six (6) wiping grooves or slots to inside of diameter of blocker ring using a jewelers file. (Figure 20, page 4-11)

Use only blocker rings with small "B" imprinted on face of ring at small end of cone. (Figure 21, page 4-11)

5. Bend tang of counter gear thrust washer to approximately 90° without cracking tang to prevent rotation of washer against case. (Figure 22, page 4-12)

6. Measure run out of counter gear rear thrust surface in case and do not use case if run out exceeds .005". (Figure 23, page 4-12)

7. Special first gear roller bearing unit (P/N 3965752) consisting of reworked 1st gear, special thrust washers and needle bearing is suggested in place of the production 1st gear. (Figure 24, page 4-12)

NOTE: The corrosion preventative coating in the needle bearing must be completely removed with either a hot degreaser or ultrasonic cleaner. The bearing should then be pressure repacked with a high temperature grease, such as shell alvania EP-2.

8. Use 20 lb. side cover detent spring (P/N 3831718). Install TCS switch plug (P/N 3906448) and gasket (P/N 3906462).

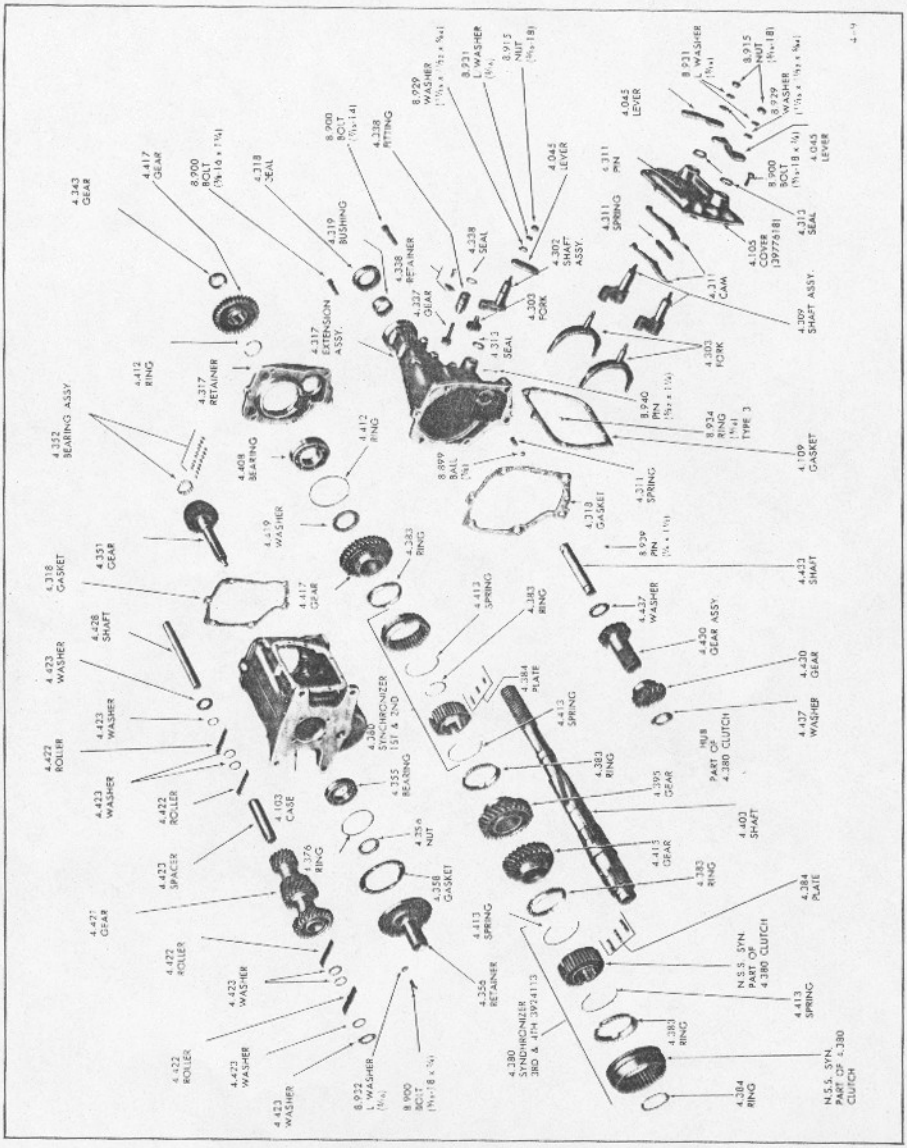


Figure 17 — Exploded view of a Muncie Four Speed transmission.

Figure 18 — Polishing front and rear surfaces of counter gear.

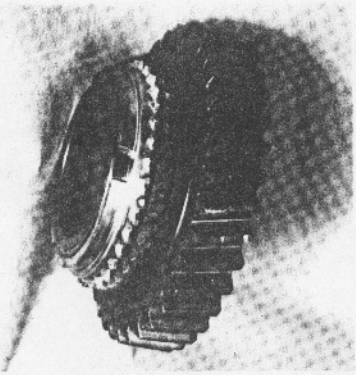


Figure 19 — Cone and blocker ring referenced in Step 4.

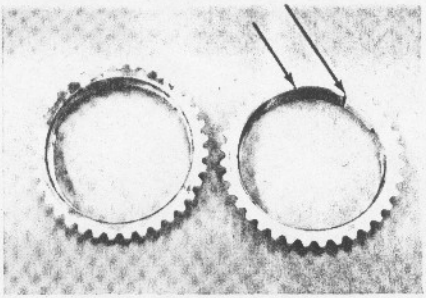


Figure 20 — Blocker ring comparison showing wiping grooves or slot modifications.

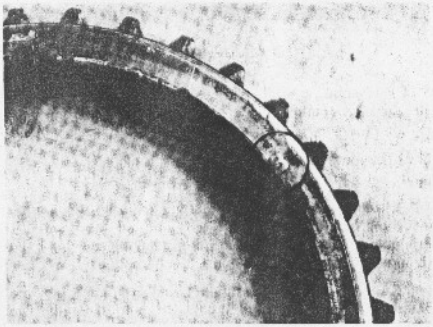


Figure 21 — Blocker ring with small "B" imprinted on ring at small end of cone.

9. Texaco 2326 multigear, D-A gear lubricant or similar high quality lubricant should be used on all running surfaces during assembly. Use magnetic fill and drain plugs.

10. Run transmission on spin test rig if possible to insure burnish of gears and proper seating of blocker rings. After transmission oil is warmed up, shift through all forward gears, slowly at first, then progressively faster.

11. Transmission side cover bolts, drain and fill plugs and clutch housing attaching bolts should all be safety wired.

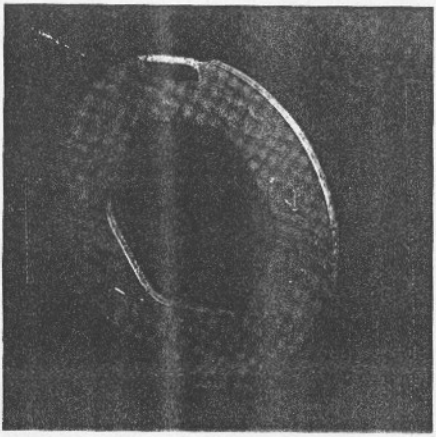
12. Some competitors use a transmission oil cooler for long distance events but may not be necessary if the transmission lubricant temperature does not exceed 275° - 290°F.

REAR AXLE AND DIFFERENTIAL

The Corvette rear axle has proven to be satisfactory for high performance applications if prescribed inspection and assembly techniques are followed.

The differential should be completely disassembled for visual and magnetic particle inspection especially for heat treatment cracks in the large or "loading" window of the differential case. Grind the edges of this large opening to a smooth contour and shot peen this surface (Figure 25, page 4-13)

The latest released differential cases P/N 3997926 for gear ratio 2.70:1 to 3.70:1 and 3997928 for gear ratio 3.90:1 to



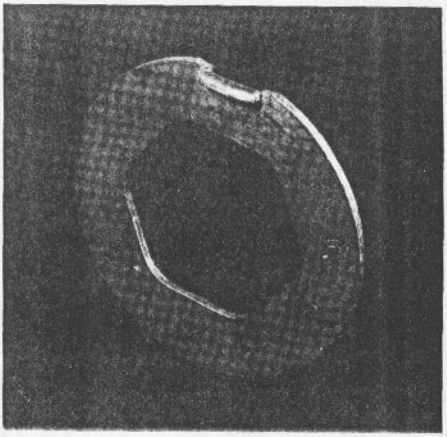


Figure 22 — Comparison of a stock washer and a washer with the desired 90° bend.

5.14:1 which have higher hardness cases should be used for all rear axle rebuilds.

Use heavy duty limited slip kit (P/N 3982240) consisting of 22 friction plates, higher load springs, thicker spring load plate and shot peened differential gears.

"Loctite bearing retainer" should be used to secure the inner and outer raceways of the differential case bearings: Production ring gear bolts and differential bearing cap bolts should also use "Loctite" stud lock compound.

Pinion bearing preload should be set to 8 to 12 in./lb. and differential bearing preload should be 13 to 15 in./lb. The shop manual procedure should be followed for reassembly of the differential.

Use pinion flange (P/N 3879208) which accepts 2-1/4" O.D. propshaft assembly (P/N 3924148). Maximum run out of pinion flange is .005 total indicated run out (T.I.R.) with zero run out desireable for high speed smoothness. Polish and prelube pinion seal contact surface.

Use axle output or drive yoke (P/N 3872922) which is cap type used on all large block vehicles. The yoke end play must be reduced to less than .005" using hardened steel shims which must be fabricated.

Use magnetic plug (P/N 2309473) in filler hole and install steel vent (P/N 3820840) in rear cover pressed into .359/.369 diameter hole. Texaco 2326 miltigear lubricant, D-A rear axle

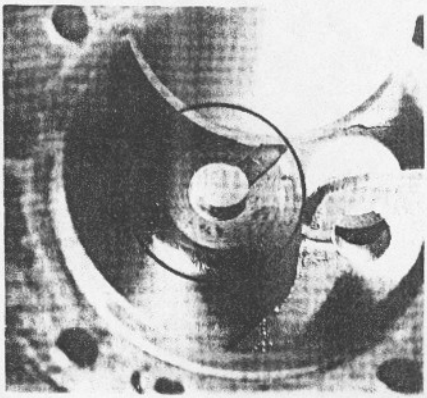


Figure 23 — Counter gear rear thrust surface.

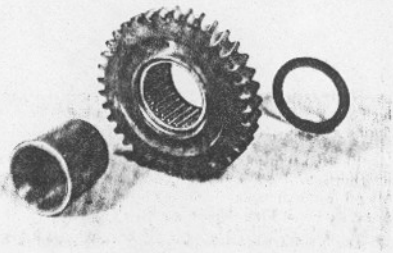


Figure 24 — Rework 1st gear, special thrust washer and needle bearing.

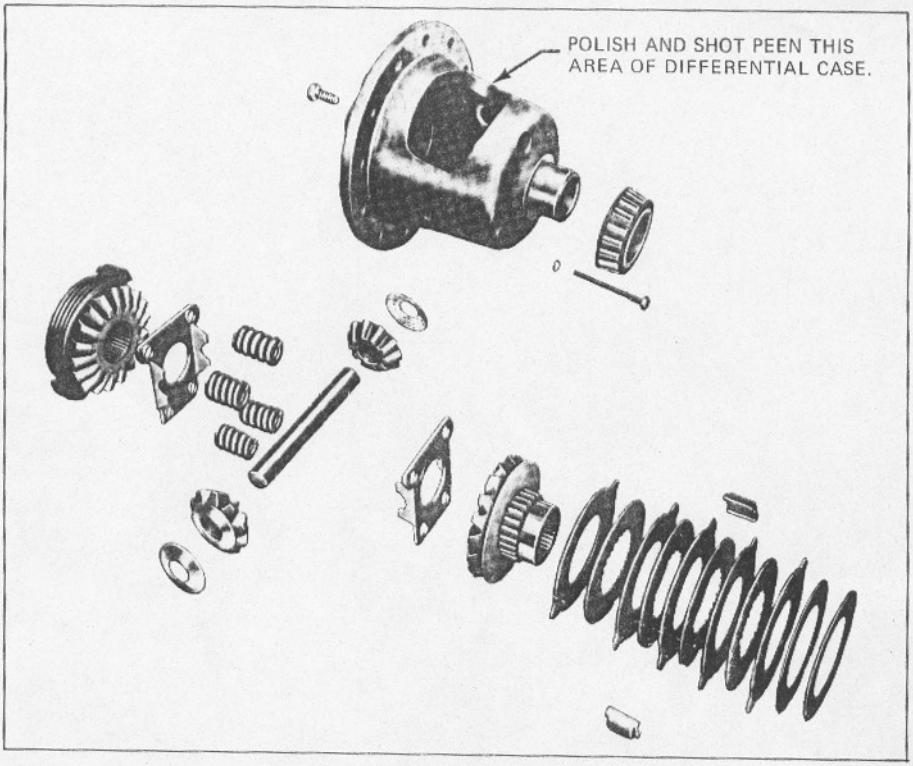


Figure 25 — Exploded view of Corvette positraction.

lubricant or other high quality lubricant should be used during build up and for running.

Gear and bearing break-in on a test stand is recommended consisting of a minimum of two (2) hours running at 1000 to 2000 RPM. The initial break-in oil should be drained and refilled for operation.

A rear axle cooler is suggested for any endurance events. Harrison oil cooler (P/N 3157804) and electric driven circulation pump such as Jabsco #6360 are frequently used by competitors. The cooler should be mounted at about the same height as the differential so the axle will not overfill in case of circulation pump failure.

Axle rear cover may be reworked for cooler lines by drilling and tapping on vertical line 3/4" to left of centerline of middle vertical rib, the top hole down 1.28" from crossmember surface and the bottom hole up 1.46" from spring surface. Tap for 1/2" pipe thread and install fittings with thread lock, grind off fittings flush with inside of cover. Lower bushing should have bottom edge of inlet rounded for oil pickup. Cooled oil enters axle through top bushing to spray on ring gear and pinion bearing.

The production differential may also be replaced by a "spool" available from several aftermarket sources which will provide a fully locked rear axle.

SUGGESTED CHEVROLET SERVICE PARTS:

NOTE: See Figure 26, page 4-14) for proper parts identification.

1. Differential Carrier Assembly (with ring & pinion)

3.08	3899132
3.70	3899137
4.11	3899139
4.56	3899141
—	3917861*

*Without Ring & Pinion

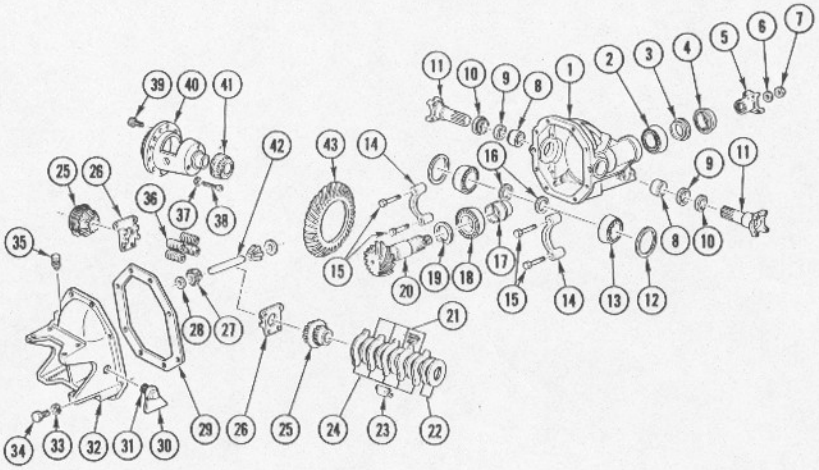


Figure 26 — 1963 - 1979 Corvette Rear Axle Carrier Assembly

1.	**CARRIER ASM.,** Differential	5.505
2.	**BEARING ASM.,** Frt. Drive Pinion	5.447
3.	**SEAL,** Pinion Flange Oil	5.469
4.	**DEFLECTOR,** Pinion Flange (Also Part of #5)	5.545
5.	**FLANGE,** W/Deflector (4 1/16" dia.)	5.545
6.	**WASHER** (1/2"-16)	8.929
7.	**NUT** (7/8"-16)	8.917
8.	**BEARING,** U Joint Yoke (Also Part of #9)	5.536
9.	**SEAL ASM.,** Diff. Carrier 7 Joint Yoke Brg. Oil	5.430
10.	**DEFLECTOR,** Diff. Carrier U Joint Yoke (Also Part of #11)	5.425
11.	**YOKE ASM.,** Diff. Carrier U Joint	5.425
12.	**BEARING,** U Joint Yoke (Also Part of #13)	5.536
13.	**BEARING,** Diff. Side	5.536
14.	**CAP** (Part of Carrier Asm.)	N.S.
15.	**BOLT,** Diff. Carrier Cap	5.506
16.	**RING,** Diff. Carrier U Joint Yoke Retaining	5.428
17.	**SPACER,** Pinion Shaft Bearing	5.453
18.	**BEARING ASM.,** Drive Pinion Rr.	5.484
19.	**SHIM,** Diff. Bearing Adjusting	5.537
20.	**GEAR ASM.,** Ring & Pinion	5.529
	NOTE 1: Part of Plate Unit 5.511.	
21.	**SPLINED DISCS** (Note 1)	5.511
22.	**SHIM**	5.511
23.	**GUIDE,** Plate	5.511
24.	**EARED DISCS** (Note 1)	5.511
25.	**GEAR,** Diff. Side	5.528
26.	**PLATE,** Pressure	5.511
27.	**GEAR,** Diff. Pinion	5.526
28.	**WASHER,** Diff. Pinion Thrust	5.542
29.	**GASKET** (9 1/64" x 10)	5.508
30.	**GASKET,** Drain Screw (1/2" I.D. x 15/16" O.D.)	5.400
31.	**PLUG,** W/Instr. Tag (1 7/64" O.L.)	5.400
32.	**COVER,** Diff. Carrier	5.398
33.	**WASHER** (7/16")	8.931
34.	**BOLT** (7/16"-14 x 1 1/4")	8.900
35.	**VENTILATOR,** Rr. Axle	5.387
36.	**SPRING,** Pressure Plate	5.511
37.	**WASHER** (5/16")	8.931
38.	**SCREW** (5/16"-18)	5.518
39.	**SCREW** (1/4"-24 x 25/32") (Used w/Lockwasher)	5.531
40.	**CASE,** Diff.	5.510
41.	**BEARING,** Diff. Side	5.536
42.	**SHAFT,** Diff. Pinion	5.517
43.	**GEAR ASM.,** Ring & Pinion (Not Sold Separately)	5.529

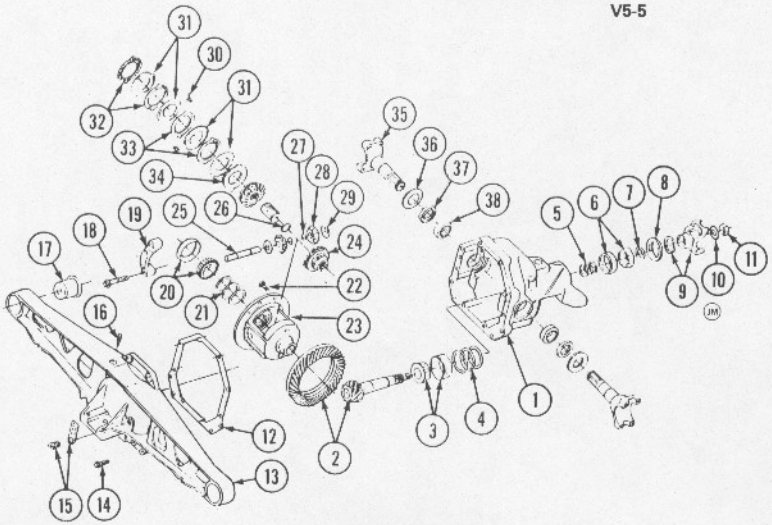


Figure 27 — 1980-82 Corvette Differential Carrier (DANA)

1.	**CARRIER,** Diff W/Cap	5.505
2.	**GEAR,** Ring & Pnc	5.528
3.	**BEARING,** Pnc	5.484
4.	**SHIM KIT,** Brg Preload Adj	5.460
5.	**SHIM** (Part of #4)	N.S.
6.	**BEARING,** Pnc Otr Preload	5.447
7.	**SLINGER,** Pnc Otr	5.484
8.	**SEAL,** Pnc Oil	5.469
9.	**FLANGE,** W/Deflector, Prop Shaft Pnc	5.545
10.	**WASHER,** Pnc Nut	5.812
11.	**NUT** (3/4"-16)	8.917
12.	**GASKET,** Diff Carrier Cover	5.399
13.	**COVER,** Diff Carrier (Carrier Mtg Mbr)	7.039
14.	**SCREW,** Carrier Cover	5.398
15.	**PLUG,** Filler	5.400
16.	**VENT,** Breather	5.387
17.	**BUSHING,** Diff Carrier Cover	5.381
18.	**SCREW,** Brg Cap	5.506
19.	**CAP** (Part of #1 Carrier)	N.S.
20.	**BEARING,** Diff	
21.	**SHIM KIT,** Brg Preload	5.537
	NOTE 1: Part of Plate Kit 5.511	
22.	**SCREW,** Ring Gear (3/8-24 x 13/16")	5.531
23.	**CASE,** Diff	5.510
24.	**GEAR,** Diff Side	5.528
25.	**SHAFT,** Pinion	5.517
26.	**RING,** Diff Side Gear Snap	5.537
27.	**RING,** Snap	N.S.
28.	**PINION,** Diff	5.526
29.	**WASHER,** Diff Pinion Thrust	5.542
30.	**RETAINER,** Diff CLutch(*1)	5.511
31.	**PLATE KIT** (*1)	N.S.
32.	**PLATE KIT** (*1)	N.S.
33.	**PLATE KIT** (*1)	N.S.
34.	**PLATE KIT** (*1)	N.S.
35.	**SHAFT,** Yoke-LH (Use W/14020911)	5.425
	SHAFT, Yoke-RH (Use W/14020911)	5.425
	SHAFT, Yoke-LH (Use W/14020912)	5.425
	SHAFT, Yoke-RH (Use W/14020912)	5.425
36.	**SHIELD,** Stone (Part of #35 Shaft)	N.S.
37.	**SEAL,** Inner Yoke	5.530
38.	**BEARING,** Yoke Inner	5.428

2. Ring and Pinion Gear Set

Ratio	Unit Part No.	Ring/ Pinion
2.73	3961431	(41/15T)
3.08	3961418	(37/12T)
3.36	3961423	(37/11T)
3.55	3961420	(32/9T)
3.70	3961419	(37/10T)
3.90	3970551	(39/10T)
4.11	3961421	(37/9T)
4.56	3961424	(41/9T)
4.88	3963840	(39/8T)
5.14	3970552	(36/7T)

3. Differential Case (Without Internal Gears)

Ratio	Part No.
All up to & incl. 3.70	3997926
4.11 4.56 3.90	3997928

4. Bearing Asm.

	Assy. No.
Diff. Side (Timken No.)	7451140
Frt. Pinion (Timken No.)	7450984
Rear Pinion (Timken No.)	7451155

5. Miscellaneous

Pinion Seal (Viton)	3982239
Pinion Flange	3879208
Diff. Conversion Kit (H.D.)	3982240
Fill Plug (Magnetic)	2309473
Drain Plug	
Vent	3820840
Oil Cooler Line Fittings (if applicable)	3220X8X6 (Weatherhead No.)

6. Recommended Bolt Torques

Ring Gear to Differential Case Bolts	40-60 lbs. ft.
Cover to Differential Carrier	35-55 lbs. ft.
Differential Cap to Carrier	50-60 lbs. ft.

BRAKE SYSTEM

The braking system is one of the main component systems on a vehicle and its proper operation is imperative to the performance of a competitive car.

There are three basic areas to the brake system: First is the correct mechanical components, second is a proper hydraulic system and bleed, and third is adequate cooling.

Components

When selecting the components for the system, a decision as to whether to use a manual brake system or a power boost system should be made. The manual brake system is generally a more responsive system but has a penalty of higher pedal effort. If a Chevrolet vacuum power boost unit is used, the noise attenuator (filter) packing around the push rod opening should be removed to improve the response time of the diaphragm during pedal application.

The production Corvette master cylinder or a similar 1-1/8" diameter master cylinder with a larger reservoir capacity can be used. An early Chevelle service master has a similar bore but larger capacity. The increased capacity insures adequate fluid for use with worn pads, such as might be encountered in a long distance race. A replacement master cylinder with 1-1/4" diameter piston can be adapted to provide increased hydraulic fluid displacement if a progressive loss of pedal height is encountered during an endurance event. These larger diameter master cylinders are used on 1974 or 75, 20 and 30 series Chevrolet trucks.

Extreme care should be taken to adjust the master cylinder push rod so the piston fully returns to rearward position when the brakes are released. The master cylinder mounting holes must also be slotted to attach to the firewall or power brake booster.

If a replacement master cylinder is used, care should be taken to remove the pressure reserve valve in the rear circuit if the master is from a disc drum type braking system. When installing the master cylinder in the car, care should be taken to insure a full stroke in the master before the pedal bottoms out of the toe pan. If this is not happening, lengthen the push rod to insure the piston bottoming out in the master cylinder bore prior to the pedal hitting the floor.

All flexible brake lines should be replaced with new production lines or with Aeroquip brand steel flexible lines. This is done in order to minimize volume change due to hose expansion during pressure applications.

When installing the rotors and caliper assemblies on the vehicle, the rotor should be centered in the caliper housing. If the production rotor and the spindle do not give this condition, then shim the rotor or caliper mount to position the rotor in the center of the caliper housing. It is useful to polish the forward edge of the opening in the caliper housing and the leading edge of the brake shoes to a smooth finish and apply a lubricant such as Molycote to reduce the friction between the pad and caliper housing during brake application.

Hydraulics

The second phase of the brake area is the bleeding of the hydraulic system. It is very essential that all the air be removed from the system in order to maintain a firm pedal under all braking applications. The most important item to remember during bleeding a brake system is to use clean, new brake fluid since brake fluid will absorb moisture from

the air which reduces the boiling point. Due to the extreme braking temperatures encountered in competition, only DOT approved 550° boiling point brake fluid should be used. Due to an affinity for moisture of any glycol base hydraulic fluid, regular fluid changes are recommended to avoid possible vapor lock of the hydraulic system under extreme temperature operation. Care should be taken when pouring the fluid into the master cylinder to prevent the formation of small bubbles which can become trapped in the system. When bleeding, either by pressure bleed system or by manually stroking the brake pedal, tap the caliper housings with a small rubber hammer to free-up any lodged air bubbles that may get caught in the calipers. Use sufficient fluid to purge the system completely and insure complete elimination of any air bubbles.

CORVETTE CHASSIS MODIFICATIONS

These specifications are intended to provide guidance in modifying a Corvette chassis and suspension for use in limited off-road gymkhana or solo slolem events. The resultant highway ride will be noticeably harsher than a production Corvette but steering response and handling will be more suitable for off-highway activities.

Chevrolet currently offers a production option RPO FE7 for the Corvette which consists of larger diameter front and rear stabilizer bars (sway bars) and higher rate (stiffer) front and rear springs.

Following is a comparison of the standard Corvette and RPO FE7 chassis components:

	Standard	1973-74 RPO FE7	1975 RPO FE7
Front Stabilizer Bar Part #	334930	3871318	351596
Front Stabilizer Bar Diameter	13/16"	7/8"	1-1/8"
Front Stabilizer Bar Bushings (2)	3923674	3923674	351595
Rear Stabilizer Bar Part #	—	3967713	351597
Rear Stabilizer Bar Diameter	—	.562"	.44"
Rear Stabilizer Bar Bushing (2)	—	480912	351600

In addition, the following front stabilizer bars are available for optimum chassis tuning.

Part Number	Bar Dia. (In.)
3831971	3/4
334930	13/16
3871318	7/8
351596	1-1/8

Stabilizer bar bushings must be fabricated for the above optional diameter bars.

Ventilation

The final facet of building a brake system is to provide adequate ventilation for the rotors. This translates into having effective air ducts to direct cooling air to the brake rotors. In the front, pull the air from the front spoiler assembly ducting it through 3" hose to the center of the rotor. The air being ducted to the center will be pumped through the vanes of the rotor by centrifugal force and provide cooling over all of the surface area of the rotor. It may be useful to direct a stream of cooling air to the surface of the rotor itself.

Ducts may be used in the rear if required. The ducting need not be as elaborate in the rear as it is in the front. This would depend upon vehicle application and drivers use of the brakes.

An adjustable brake pressure regulator valve (P/N 3878944) is recommended for the rear brake hydraulic system to prevent rear brake lock up. The adjustment procedure and limitations for the brake proportioning valve are shown in figure 28.

The following chassis springs are also available:

	Part Number	Rate #/In.
Production Front	346939	280
RPO F41 Front	3832518	550*
"Daytona" Front	3986032	860*

*Lowers vehicle approximately one inch.

Production Rear	356825	140
F-41 Rear	354130	305

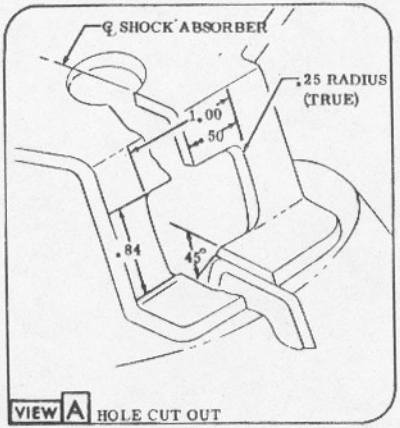
Suggested initial suspension geometry settings for the Corvette are:

Front and Rear Camber	−1° ± 1/2°
Front Caster	+1° ± 1/2°
Toe in (Total)	1/16" ± 1/32"

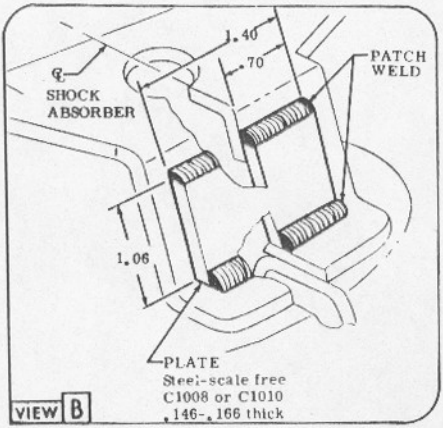
Optimum front and rear suspension settings are normally determined by tire tread wear patterns and by measuring the operating temperature profile if a tire pyrometer is available. Manufacturers recommendations should be followed for tire inflation pressure.

Multi-position adjustable shock absorbers available from several after-market suppliers are generally recommended. Clearance between optional shock absorbers and suspension coil springs and spring tower should be carefully checked during full wheel travel. Spring tower modification and bump rubber rework for Koni shock absorber clearance as shown.

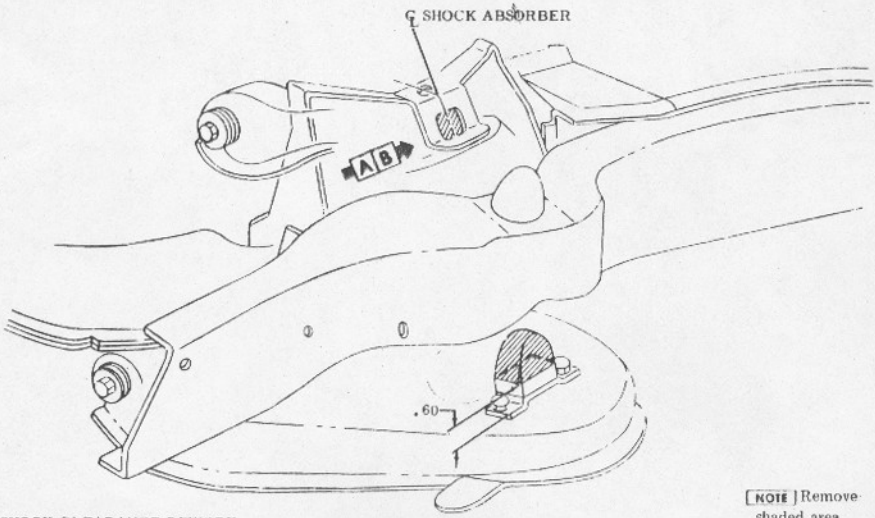
HEAVY DUTY SUSPENSION INSTRUCTION SHEET
CORVETTE



VIEW A HOLE CUT OUT



VIEW B



SHOCK CLEARANCE REWORK

SEE SHOP MANUAL FOR REMOVAL AND INSTALLATION OF SHOCKS AND SPRINGS.

Figure 28

INSTRUCTIONS

ADJUSTABLE PROPORTIONING VALVE

This new adjustable proportioning valve is designed to control front-to-rear brake balance on race cars and other off-road vehicles. Applications include oval dirt-track cars, stock cars, road course sports cars and any other type of racing where track surface conditions vary, necessitating a change in brake system balance.

The valve is easily adjusted, no tools required, and features an infinitely variable "knee point" from 100 to 1000 psi.

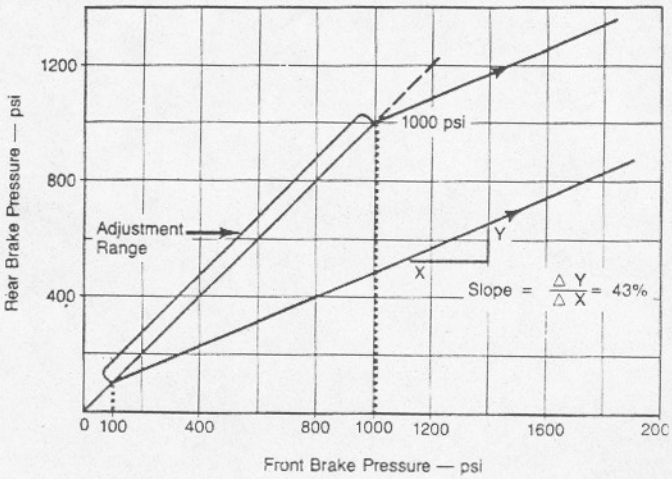
INSTALLATION

To achieve the maximum benefit from the adjustable feature, it is recommended that the valve be installed inside the car, within easy reach of the driver. Mounting attitude is not critical. The valve should be plumbed into the rear brake system, between the master cylinder and rear brakes, downstream of any differential warning valve, if one is present. The brake tube from the master cylinder is connected to the top port on the valve (marked "IN"), and the rear brake tube connected to the bottom port (marked "OUT"). Both the inlet and outlet ports are tapped for 7/16-24 tube nuts and will accept 3/16 or 1/4" O.D. flared tubing. Mounting is accomplished by using the two 5/16" holes.

ADJUSTMENT

Brake balance is varied by rotating the adjusting knob: clockwise to increase rear brakes, counterclockwise to decrease rear brakes. It is recommended that the valve be initially set at its minimum position (fully-counterclockwise), and be adjusted upwards, under actual track conditions, until the front-to-rear balance is optimum. After this initial setting is made, only fine tuning will be required to compensate for varying track surface conditions.

Caution:	Do not use copper tubing, copper can flex and break. Steel tubing with double flared ends is recommended. (SAE specification J527 and FMVSS 116.)



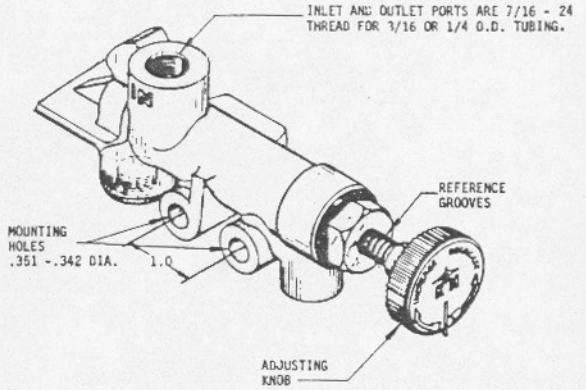


Figure 29 — Adjustment Procedure for Brake Proportioning Valve

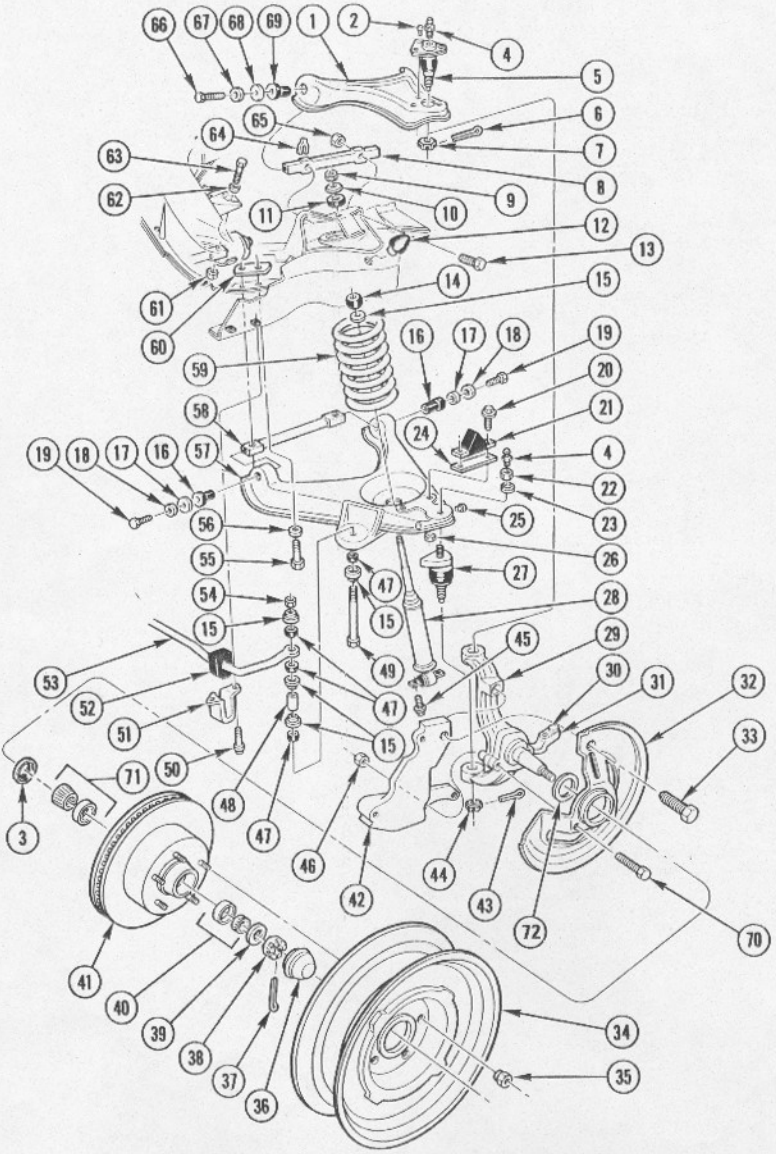


Figure 30 — 1965-82 Corvette Front Suspension (Typical)

1970-82 Corvette Front Suspension (Typical)

1.	**ARM ASM.,** Upper Steering Knuckle	6.168
2.	**RIVET** (1/4" x 3/4")	8.967
3.	**SEAL ASM.,** Frt. Whl. Brg.	6.326
4.	**FITTING** (1/4"—28)	8.984
5.	**STUD UNIT,** Upper Ball Replacement	6.164
6.	**PIN,** Cotter (1/8" x 1")	8.938
7.	**NUT** (1/2"—20)	8.917
8.	**SHAFT,** Unit, Upper Control Arm	6.164
9.	**NUT** (3/8"—14)	8.915
10.	**WASHER,** Frt. Upr. Abs. (13/32" I.D. x 1 29/32" O.D.)	7.347
11.	**GROMMET,** Frt. Upr. Abs. Link	7.388
12.	**BUMPER,** Upr. Control Arm	6.176
13.	**BOLT,** Upr. Control Arm (7/16"—13 x 2 1/4")	6.178
14.	**GROMMET,** Frt. Absorber	N.S.
15.	**RETAINER,** Stab. Link Grommet	7.389
16.	**BUSHING,** Lwr. Control Arm (1 13/16" I.D. x 23/32" O.D.)	6.170
17.	**RETAINER,** Bushing (7/16" I.D. x 1 13/16" O.D.)	6.170
18.	**WASHER** (7/16" x 51/64" x 5/32")	8.931
19.	**BOLT** (7/16"—20 x 1 1/8")	8.900
20.	**SCREW** (5/16"—18 x 1)	8.977
21.	**BUMPER,** Lwr. Control Arm	6.176
22.	**NUT** (1/2"—20)	N.S.
23.	**WASHER** (1/2" x 7/8")	8.931
24.	**SPACER,** Lwr. Control Arm Bumper	6.176
25.	**RIVET,** Strg. Knu. Lwr. Control Arm (5/16"—1)	N.S.
26.	**NUT** (5/16"—18)	8.915
27.	**STUD UNIT,** Lower Ball Replacement	6.174
28.	**ABSORBER UNIT,** Front	7.345
29.	**KNUCKLE ASM.,** Steering	6.020
30.	**RIVET,** Strg. Knu. Arm Rr. Hole Plug (Part of #32)	N.S.
31.	**ARM,** Steering Knuckle	6.103
32.	**SHIELD,** Front Disc Splash	5.002
33.	**BOLT,** Frt. Caliper Adapter Brkt. (11/16"—16 x 7/8")	5.002
34.	**WHEEL ASM.,** Front	5.803
35.	**NUT,** Wheel Hub Bolt (7/16"—20)	5.813
36.	**GREASE CAP,** Frt. Wheel Bearing (2 5/64" O.D.)	6.330
37.	**PIN,** Cotter (1/8" x 1 1/2")	8.938
38.	**NUT,** Steering Knuckle Spindle (27/32"—20)	6.022
39.	**WASHER,** Steering Knuckle Spindle (1 13/32" O.D.)	6.321
40.	**BEARING ASM.,** Frt. Wheel Outer	6.313
41.	**DISC,** Front Wheel Brake	5.809
42.	**BRACKET,** Caliper Adapter	5.001
43.	**PIN,** Cotter (1/8" x 1)	8.938
44.	**NUT** (5/16"—18)	8.917
45.	**SCREW** (5/16"—18 x 1")	8.900
46.	**NUT** (1/2"—20)	8.917
47.	**GROMMET,** Stabilizer Link (Rubber 13/32" I.D. x 1 O.D.)	7.244
48.	**SPACER,** Bolt Grommet	7.240
49.	**BOLT,** Frt. Stab. Shaft Link (5/16"-24 x 5 29/32")	7.240
50.	**SCREW** (5/16"—18 x 1 7/16")	8.900
51.	**BRACKET,** Shaft Front, Rear Frame	7.242
52.	**BUSHING,** Shaft Front Frame	7.243
53.	**SHAFT,** Stabilizer Front	7.241
54.	**NUT,** Stabilizer Link (5/16"—24)	7.238
55.	**BOLT,** Strg. Knu. Lwr. Arm	N.S.
56.	**WASHER** (7/16" x 51/64" x 5/32")	8.931
57.	**ARM ASM.,** Lower Steering Knuckle	6.168
58.	**SHAFT,** Unit, Lower Control Arm	6.169
59.	**SPRING ASM.,** Front	7.412
60.	**NUT,** Lower Arm (7/16"—20)	6.172
61.	**NUT** (5/16"—18)	8.917
62.	**WASHER,** Lower Arm (27/64")	6.172
63.	**BOLT** (7/16"—18 x 2 5/32")	N.S.
64.	**SHIM,** Upper Arm (7/16")	6.178
65.	**NUT** (7/16"—14)	6.917
66.	**BOLT** (3/8"—24 x 7/8")	8.900
67.	**WASHER** (3/8")	8.931
68.	**RETAINER** (Hole 3/4" I.D.)	6.163
69.	**BUSHING** (1 1/2" O.D. x 1 1/2" O.L.) Upper Cont. Arm	6.164
70.	**BOLT** (11/16"-16 x 7/8")	5.002
71.	**BEARING ASM.,** Inner	6.311
72.	**GASKET,** Splash Shield	6.002

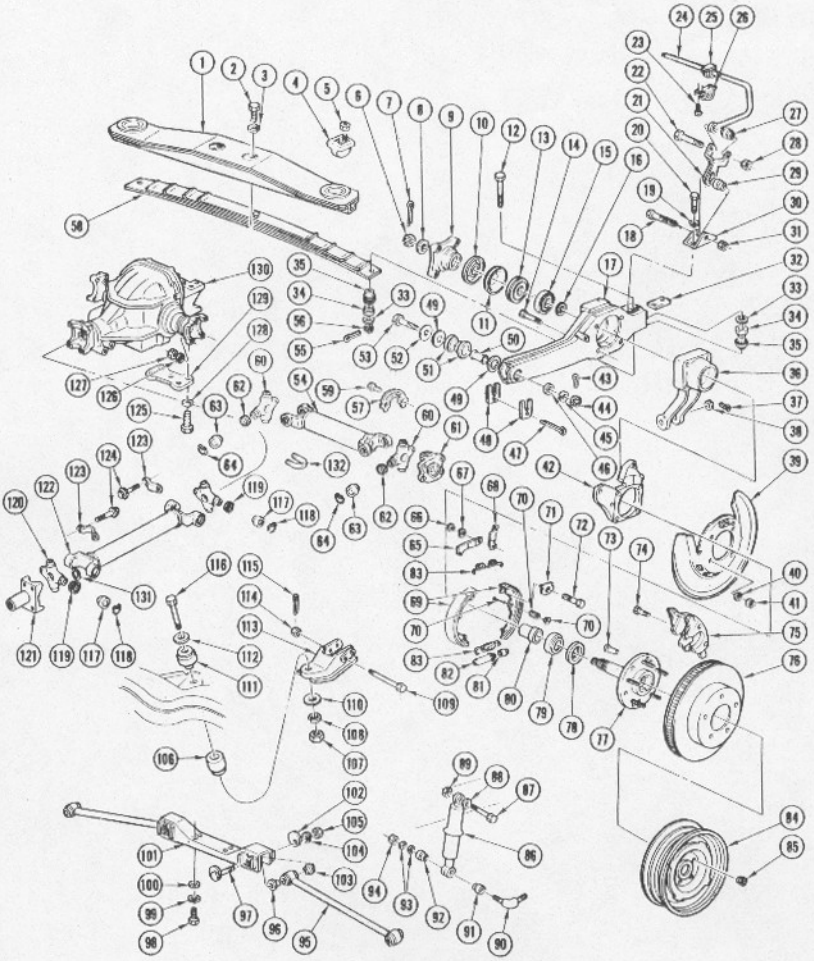


Figure 31 — 1963-79 Corvette Rear Suspension (Typical)

1963-79 Corvette Rear Suspension (Typical)

1. **MEMBER,** Rr. Susp. Mtg. Cross 5.380
2. **BOLT** (7/16"-14 x 1 1/4") 8.900
3. **WASHER** (7/16") 8.929
4. **BUMPER,** Rr. Spring 7.535
5. **NUT** (3/4"-16) 6.760
6. **NUT** (3/4"-20) 5.829
7. **PIN,** Rr. Whl. Spindle 8.939
8. **WASHER** (7/32") 5.830
9. **FLANGE,** Rr. Whl. Spindle 5.806
10. **DEFLECTOR,** Rr. Whl. Spindle 5.425
11. **SHIELD,** Rr. Whl. Inr. Brg. 5.822
12. **BOLT,** Rr. Spring 7.529
13. **SEAL,** Rr. Inr. Brg. 5.822
14. **BOLT** (5/8"-24 x 2 7/8") 5.002
15. **BEARING,** Rr. Whl. Inr. 5.855
16. **SHIM,** Rr. Whl. 5.855
17. **ARM,** Torque Control 5.382
18. **BOLT** (5/8"-16 x 1 3/4") 8.900
19. **WASHER** (9/16" x 15/32") 8.931
20. **BOLT** (9/16"-18 x 3/4") 8.900
21. **LINK,** Rr. Stab. Shaft 5.415
22. **BOLT** (3/4"-16 x 1 3/4") 8.900
23. **BOLT** (9/16"-18 x 1 7/16") 8.900
24. **SHAFT,** Rr. Stab. 7.241
25. **BUSHING,** Rr. Stab. Shaft 7.243
26. **BRACKET,** Stab. Shaft 7.242
27. **BUSHING,** Rr. Stab. 7.243
28. **NUT** (3/4"-16) 8.916
29. **BUSHING,** Rr. Stab. 7.243
30. **BRACKET,** Rr. Stab. Shaft 7.242
31. **NUT** (3/4"-16) 8.916
32. **PLATE** (3 x 3/4" w/2 Holes) 7.245
33. **RETAINER,** Otr. Rr. Spring 7.545
34. **CUSHION,** Rr. Spring 7.545
35. **RETAINER,** Inner 7.545
36. **SUPPORT,** Cont. Arm to Spindle 5.380
37. **PIN** (5/32"-1 1/4") 8.938
38. **NUT** (3/4"-18) 8.917
39. **PLATE,** Parking Brake 5.001
40. **WASHER** (3/8") 8.931
41. **NUT** (3/4"-24) 8.915
42. **BRACKET,** Brake Flange Rr. 5.001
43. **PIN** (3/32" x 1") 8.938
44. **NUT** (7/16") ..... N.S.
45. **WASHER** (7/16" x 51/64" x 5/32") 8.931
46. **WASHER** (15/32") 2.277
47. **PIN** (11/64" x 4 1/4") 8.938
48. **SHIM,** Rr. Susp. Torque 5.380
49. **PLATE,** Rr. Susp. Torque 5.381
50. **RETAINER,** Torque Arm Bshg. 5.381
51. **BUSHING,** Upr. Rr. 5.381
52. **WASHER** (15/32") 2.277
53. **BOLT** (7/16"-20 x 49/64") 5.383
54. **SHAFT,** Axle 5.420
55. **PIN** (1/4" x 1 1/4") 8.938
56. **NUT** (9/16"-18) 8.917
57. **LOCK,** Rr. Axle U-Joint 5.428
58. **SPRING,** Leaf Type Rr. 7.503
59. **BOLT** (7/16"-20 x 1 1/8") 8.900
60. **REPAIR KIT,** Rr. Whl. Dr. Shaft U-Joint 5.428
61. **FLANGE,** Rr. Whl. U-Joint 5.425
62. **SEAL,** Yoke Brg. (1 13/64" x 23/32") (Also Part of #60) 5.430
63. **BEARING,** U-Joint (Also Part of #60) 5.428
64. **LOCK RING,** U-Joint (Also Part of #60) 5.586
65. **LEVER ASM.,** Parking Brake 5.149
66. **CLIP,** Parking Lvr. 5.056
67. **WASHER,** Pivot Pin 5.158
68. **STRUT,** Parking Brake 5.150
69. **SHOE UNIT,** Parking Brake 5.017
70. **PIN,** Brake Shoe 5.043
71. **PLATE,** Brake Shoe Guide 5.045
72. **BOLT** (1/2"-20 x 2") 5.056
73. **RIVET,** Spindle ..... N.S.
74. **BOLT** (7/16"-20 x 1 1/8") 8.900
75. **HOUSING ASM.,** Brake Caliper 4.665
76. **DISC,** Rr. Whl. Brake 5.809
77. **SPINDLE,** Rr. Whl. 5.806
78. **SEAL,** Rr. Whl. Otr. Brg. 5.822
79. **BEARING ASM.,** Rr. Whl. Otr. 5.855
80. **SPACER,** Brg. 5.816
81. **SOCKET,** Adj. Screw 5.111
82. **SCREW,** W/Nut Parking Brake 5.110
83. **SPRING,** Parking Brake 5.026
84. **WHEEL ASM.,** Rr. 5.803
85. **NUT** (7/16"-20) 5.813
86. **ABSORBER UNIT,** Rr. Shock 7.345
87. **BOLT** (7/16"-20 x 2 9/16") 7.347
88. **WASHER** (7/16") 8.931
89. **NUT** (7/16"-20) 8.916
90. **SHAFT,** Rr. Whl. Spindle 5.381
91. **GROMMET,** Shk. Abs. Link Rr. Lwr. 7.388
92. **GROMMET,** Shk. Abs. Link Rr. 7.388
93. **WASHER** (1/2" x 7/8") 8.931
94. **NUT** (1/2"-20) 5.383
95. **STRUT,** Rr. Whl. Spindle Supt. 5.415
96. **CAP,** Bushing 6.170
97. **CAM,** Spindle Supt. Strut 5.417
98. **BOLT** (3/4"-16 x 7/8") 8.900
99. **WASHER** (3/4") 8.931
100. **WASHER** (23/32") 8.929
101. **BRACKET,** Rr. Whl. Spindle 5.380
102. **CAM,** Rr. Whl. Spindle 6.172
103. **CAP,** Bushing 6.170
104. **WASHER** (1/2") 8.931
105. **NUT** (1/2"-20) 5.382
106. **CUSHION,** Diff. Carrier Mtg. 9.023
107. **NUT** (7/16") 8.915
108. **WASHER** (7/16") 8.931
109. **BOLT** (7/16"-20 x 4 13/16") 6.383
110. **PLATE,** Body Mtg. Cushion ..... N.S.
111. **CUSHION,** Diff. Carrier Mtg. 9.023
112. **WASHER** (7/16") 8.154
113. **BRACKET,** Diff. Carrier Mtg. 5.500
114. **NUT** (7/16"-20) 0.002
115. **PIN** (3/32" x 1") 8.938
116. **BOLT** (7/16"-14 x 2 7/8") 8.900
117. **BEARING ASM.** (Part of #120) 5.548
118. **RING,** U-Joint (Also Part #120) 5.586
119. **SEAL,** U-Joint (Also Part #120) 5.560
120. **REPAIR KIT,** U-Joint 5.548
121. **YOKE,** U-Joint Sleeve Frt. 5.555
122. **SHAFT,** Rr. Propeller 5.544
123. **STRAP,** Propeller Shaft 5.586
124. **BOLT** (5/16"-24 x 1 7/16") 5.586
125. **BOLT** (5/16"-12 x 3 31/64") 8.900
126. **WASHER** (3/4" x 45/64") 8.931
127. **NUT** (9/16"-24) 8.915
128. **WASHER** (9/16") 8.929
129. **PLATE,** Rr. Spring Anchor 7.518
130. **CARRIER ASM.,** Diff. 5.505
131. **SPACER,** Trunnion Brg. 5.566
132. **"U" BOLT** (Trunnion Finger Dia. 25/32") 5.586

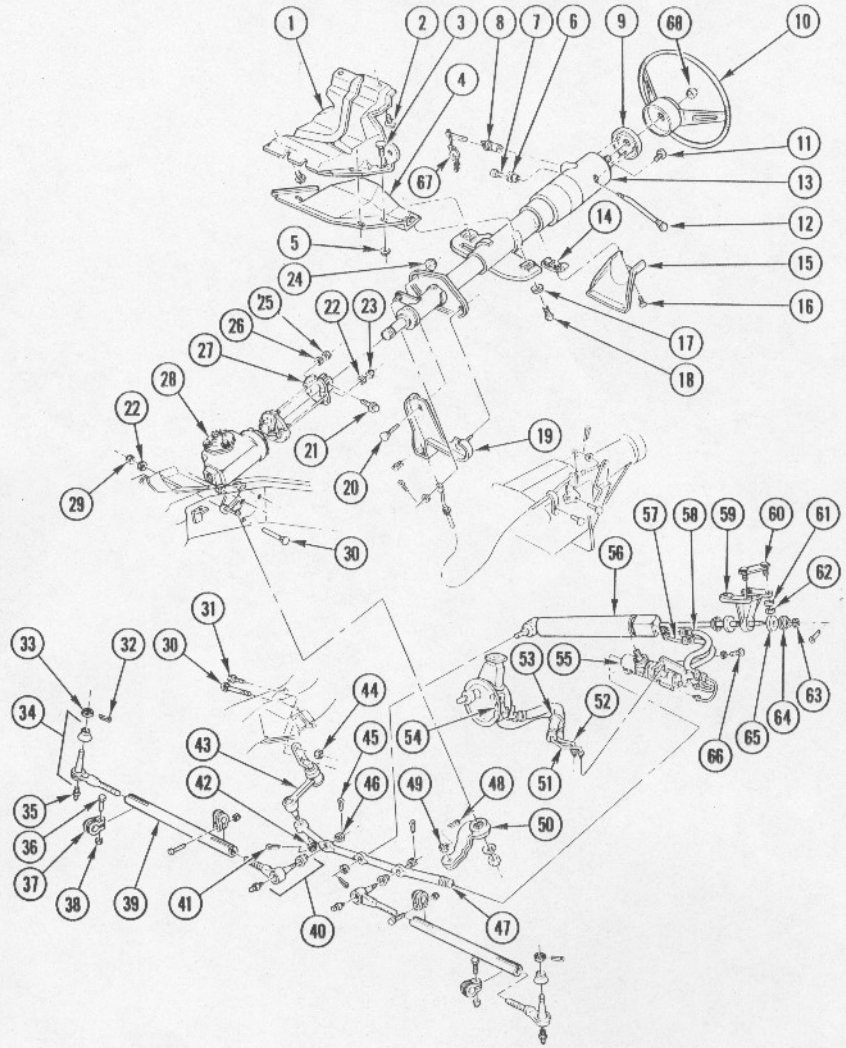


Figure 32 — Corvette Steering Column & Related Parts (Typical)

Corvette Steering Column & Related Parts

1.	**BRACKET,** Strg. Column Supt.	6.760
2.	**SCREW ASM.,** Strg. Col. Supt. Brkt. (5/16"—18 x 1")	N.S.
3.	**BOLT,** Strg. Column Supt. Brkt.	N.S.
4.	**SUPPORT ASM.,** Strg. Column	6.760
5.	**NUT** (5/16"—18)	8.917
6.	**KNOB ASM.,** Traffic Hazard Warning	2.575
7.	**SCREW,** Knob to Strg. Column	8.909
8.	**LOCK SET PKG.,** Ignition	2.188
9.	**COVER,** Strg. Col. Shift Lk.	6.500
10.	**WHEEL ASM.,** Steering	6.513
11.	**BOLT** (#8—32 x 3/8")	8.977
12.	**LEVER,** w/Knob, Direct Signal Lamp	2.897
13.	**COLUMN ASM.,** Steering	6.518
14.	**RETAINER,** Trans. Cont. Shaft	2.698
15.	**COVER,** I/P Lwr. Strg. Column (Paint to Match)	6.518
16.	**SCREW,** I/P Lwr. Strg. Col. Cvr. (#8—18 x 1/2")	9.262
17.	**WASHER,** Strg. Col. Supt	N.S.
18.	**BOLT** (3/8"-16 x 1")	8.900
19.	**BRACKET ASM.,** Trans. Cont. Lk.	4.070
20.	**BOLT** (5/16"—18 x 1 1/4")	N.S.
21.	**BOLT,** Steering Shaft U-Joint (3/8"—24 x 1 7/32")	6.525
22.	**WASHER** (3/8" x 49/64")	8.931
23.	**NUT** (3/8"—24)	8.915
24.	**NUT** (5/16"—18)	8.917
25.	**NUT** (5/16"—24)	8.915
26.	**WASHER** (5/16" x 19/32")	8.931
27.	**FLANGE ASM.,** Strg. Gear Shaft Upr.	6.525
28.	**GEAR ASM.,** Steering	6.508
29.	**NUT** (3/8"—24)	8.915
30.	**BOLT,** Strg. Gear to Frame (3/8"—24 x 4 1/2")	6.509
31.	**BOLT,** Strg. Relay & Tie Rod to Frame (3/8"—24 x 1 1/8")	N.S.
32.	**PIN** (3/32" x 1")	8.938
33.	**NUT,** Strg. Relay & Tie Rod to Strg. Knu. (5/16"—20)	N.S.
34.	**END ASM.,** Tie Rod Outer	6.233
35.	**FITTING,** Lubricant	8.984
36.	**BOLT** (5/16"—18 x 1 1/4")	8.900
37.	**CLAMP,** Tie Rod	6.234
38.	**NUT** (5/16"—18)	8.917
39.	**SLEEVE,** Frt. Axle Tie Rod	6.232
40.	**END ASM.,** Tie Rod Inner	6.233
41.	**PIN** (3/32" x 1")	8.938
42.	**NUT** (1/2"—20)	8.917
43.	**LEVER ASM.,** Steering Idler	6.896
44.	**NUT** (3/8"—24)	8.917
45.	**PIN,** Cotter (3/32" x 3/4")	8.938
46.	**NUT** (7/16" x 20—30)	8.917
47.	**ROD ASM.,** Steering Relay	6.870
48.	**PIN** (1/8"—1)	8.938
49.	**NUT** (1/2"—20)	8.915
50.	**ARM,** Pitman	6.859
51.	**HOSE,** Pump to Control Valve Inlet	6.670
52.	**HOSE,** Pump to Control Valve Outlet	6.671
53.	**CLAMP,** Strg. to Gear Inlet & Outlet	6.672
54.	**CLAMP,** Hydr. Strg. Pump to Cont. Valve Outlet Hose	6.672
55.	**VALVE,** Strg. Gear Hydr. w/Adapter	6.550
56.	**CYLINDER,** Strg. Gear Hydr. (23 3/64" O.L.)	6.576
57.	**PIPE,** Strg. Cont. Valve to Extend Cyl.	6.670
58.	**PIPE,** Strg. Cont. Valve to Retract Cyl.	6.670
59.	**BRACKET,** Frame, Hydr. Strg. Cyl.	6.575
60.	**REINFORCEMENT,** Frame Brkt.	6.575
61.	**WASHER** (5/16"—13/32")	8.931
62.	**NUT** (5/16"—18)	8.915
63.	**NUT,** Frame Brkt. (3/8"—24)	6.575
64.	**WASHER,** Hydr. Strg. Cyl. to Frame	6.581
65.	**GROMMET,** Hydr. Strg. Cyl. to Frame Brkt.	6.581
66.	**SPACER,** Hydr. Strg. Cyl. to Frame Brkt.	6.581
67.	**KEY,** Blank	2.187
68.	**NUT,** Hex. (5/16"-18)	8.916

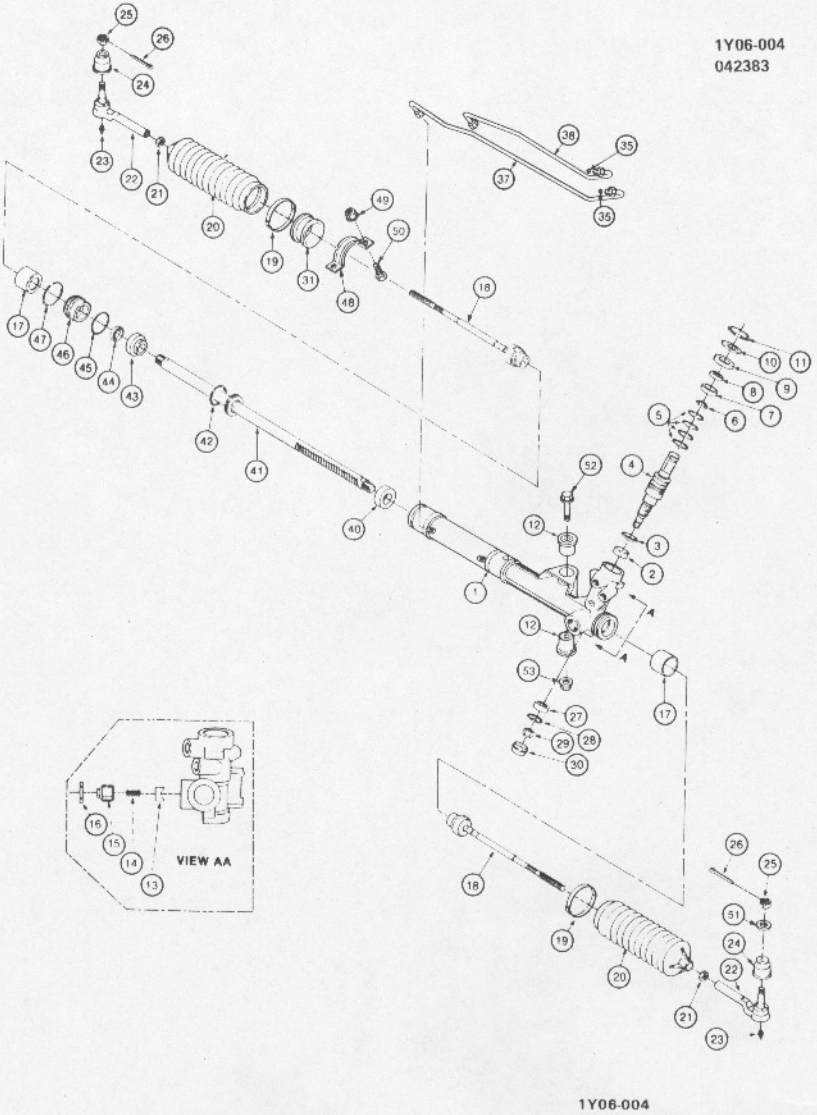


Figure 33 — 1984 "Y" RACK AND PINION STEERING -POWER

1Y06-004

1984 "Y" RACK AND PINION STEERING - POWER

1.	6.803	**HOUSING,** Rack & Pinion Asm	7841632
2.	N.S.	**BUSHING,** Upper Pinion (Part of #1)(*6)	
3.	N.S.	**SEAL,** Pinion Shaft (Part of #1 & 4)(*2,3,6)	
4.	6.550	**VALVE,** Pinion Asm (Exc FE7)(Incls #2,3,5,9,10)	7841620
	6.550	**VALVE,** Pinion Asm (W/FE7)(Incls #2,3,5,9,10)	7841621
5.	N.S.	**RING,** Vlv Bdy (Part of #4)	
6.	6.552	**RING,** Ret, Vlv Bdy Rings	7831197
7.	N.S.	**ANNULUS,** Stub Shaft Brg(*3)	
8.	N.S.	**BEARING,** Hyd Strg Gear, Ndl(*3)	
9.	N.S.	**SEAL,** Stub Shaft (Part of #4)(*2,3,6)	
10.	N.S.	**SEAL,** Stub Shaft Dust(*2,3,6)	
11.	6.552	**RING,** Ret Stub Shaft Seal	7828017
12.	6.786	**BUSHING KIT,** Mounting	7841634
13.	6.786	**BEARING,** Rack	7827995
14.	6.822	**SPRING,** Adjuster (R&P)	7838791
15.	6.822	**PLUG,** Adjuster	7841283
16.	6.822	**NUT,** Adjuster Plug Lock	7841284
17.	6.865	**RING,** Shock Dampener	7841291
18.	6.230	**ROD KIT,** Inner Tie (Incls #19,17,21)	7841627
19.	6.242	**CLAMP,** Boot	7841287
20.	6.242	**BOOT KIT,** (R & P) (Incl #19)	7841628
21.	N.S.	**NUT,** Hex (M14 x 1.5) (Part of #48)	
22.	6.233	**ROD KIT,** Otr Tie (Includes #23,24,25,26)	7841637
23.	N.S.	**FITTING,** Lube (Part of #22)	
24.	6.242	**SEAL,** Tie Rod	7840726
25.	6.509	**NUT,** Hex Slotted (M12 x 1.25 x 13)	14050089
26.	8.938	**PIN,** Cotter (1/8" x 1 1/2")	
27.	N.S.	**BEARING,** Pinion Ball (Part of #1)(*2)	
28.	N.S.	**RING,** Retaining (Part of #1)	
29.	8.917	**NUT,** Hex Lock (M10 x 1.5)	
30.	6.824	**COVER,** Dust	7828065
31.	6.509	**GROMMET,** Mounting	7840293
35.	6.674	**SEAL,** O-Ring, Cyl Line	7828486
37.	6.670	**LINE KIT,** Cylinder LH (Includes #35)	7841625
38.	6.670	**LINE KIT,** Cylinder RH (Includes #35)	7841626
40.	N.S.	**SEAL,** Inner Rack(*4)	
41.	6.586	**RACK,** W/Piston and Nut (Exc FE7)	7840286
	6.586	**RACK,** W/Piston and Nut (W/FE7)	7841042
42.	N.S.	**RING,** Piston(*4)	
43.	N.S.	**BULKHEAD,** Cylinder Inner(*1)	
44.	N.S.	**SEAL,** Outer Rack(*1,4,5)	
45.	N.S.	**SEAL,** O-Ring(*1,4,6)	
46.	N.S.	**BULKHEAD,** Cylinder Outer(*1)	
47.	6.509	**RING,** Bulkhead Retaining(*4,5)	
48.	6.509	**CLAMP,** Strg Gr to Frame	14048997
49.	8.917	**NUT,** Hex Prev Torq (M8 x 1.25)(10)	11500760
	8.929	**WASHER,** Flat (M8 ID 24 OD 2.3 Thk)	11500323
50.	8.900	**BOLT,** Hex (M8 x 1.25 x 20)	11508687
51.	6.509	**WASHER,** Strg Gear Mtg (6.164)	14047779
52.	8.900	**BOLT,** Hex Flg Hd (M10 x 1.5 x 85)(10.9)	
53.	8.917	**NUT,** Hex Wa Torq Prev (M10 x 1.5)(10)	

NOTE 1: Part of #7841624 BULKHEAD ASM, Service Kit Grp 6.855.
NOTE 2: Part of #7838278 BEARING KIT, Pinion Grp 6.525.
NOTE 3: Part of #7838279 BEARING KIT, Stub Shaft Grp 6.525.
NOTE 4: Part of #7841629 SEAL KIT, Rack and Pinion Grp 6.579.
NOTE 5: Part of #7841623 SEAL KIT, Bulkhead Grp 6.855.
NOTE 6: Part of #7841622 SEAL KIT, Pinion and Shaft Grp 6.525.

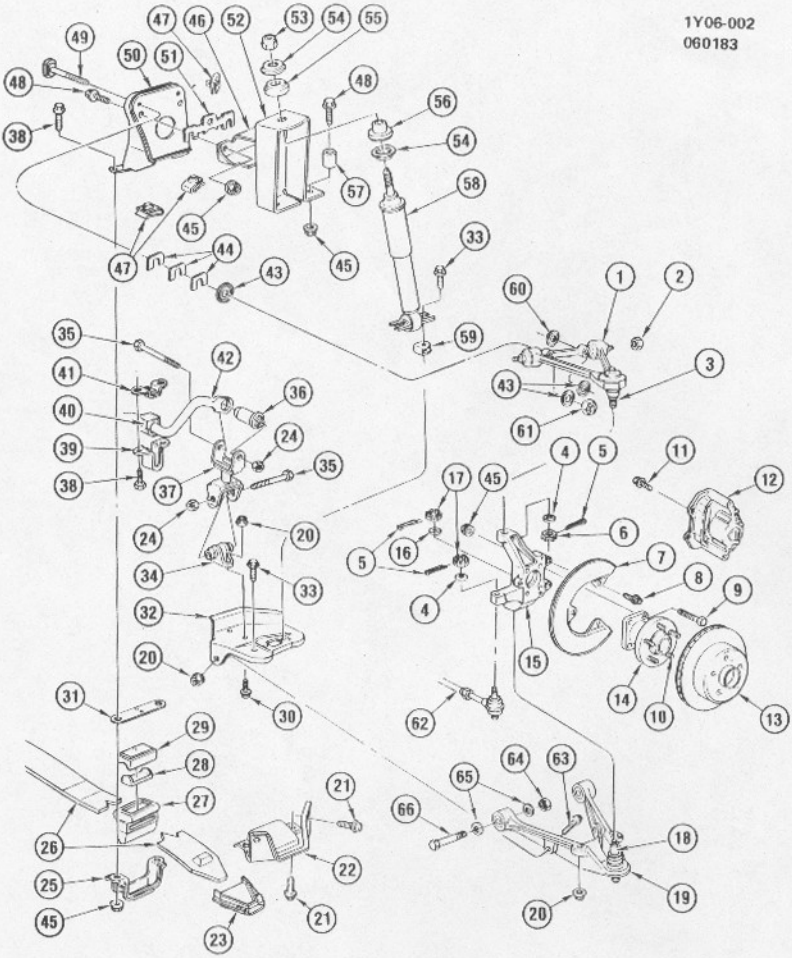


Figure 34 — 1984 "Y" FRONT SUSPENSION

1Y06-002

1984 "Y" FRONT SUSPENSION

1.	6.168	**ARM,** Frt Upr Cont-LH (Incl #2, 3, 4, 5, 6)	14067597
	6.168	**ARM,** Frt Upr Cont-RH (Incl #2, 3, 4, 5, 6)	14067598
2.	6.164	**NUT,** Strg Knu Upr Cont Arm	14060153
3.	6.164	**BALL STUD,** Strg Knu Upr Cont Arm	9769596
4.	6.164	**WASHER,** Strg Knu Upr Cont Arm Ball Stud	14047779
5.	8.938	**PIN,** Cotter (1/8" x 1 1/2")	
6.	6.164	**NUT,** Strg Knu Upr Cont Arm Ball Stud (5.418)	14050089
7.	5.002	**SHIELD,** Frt Brk	14046905
8.	8.977	**SCREW,** Hex W/Fl Wa (M6.3 x 1.81 x 16)	
9.	8.900	**BOLT,** Hex (M10 x 1.5 x 65)	
10.	5.812	**BOLT,** Frt Whl Mtg	9590476
11.	4.665	**BOLT,** Frt Brk Clpr Mtg	14066904
12.	N.S.	**CALIPER,** Frt Brk(*1)	
13.	5.809	**ROTOR,** Frt Brk	14055933
14.	6.307	**HUB,** Frt Whl (Incl #10)	7466923
15.	6.020	**KNUCKLE,** Strg-LH	14035699
	6.020	**KNUCKLE,** Strg-RH	14035700
16.	6.178	**WASHER,** Strg Knu Lwr Cont Arm Ball Stud	14047778
17.	6.164	**NUT,** Strg Knu Lwr Cont Arm Ball Stud	14048922
18.	6.174	**BALL STUD,** Strg Knu Lwr Cont Arm	9769599
19.	6.168	**ARM,** Frt Lwr Cont-LH (Incl #5, 16, 17, 18)	14067623
	6.168	**ARM,** Frt LWr Cont-RH (Incl #5, 16, 17, 18)	14067624
20.	8.915	**NUT,** Hex Flg (M8 x 1.25)	
21.	7.010	**BOLT,** Frt C/Mbr, Hex Flg Hd (M8 x 1.25 x 20) (3.671)	14060613
22.	7.420	**PROTECTOR,** Frt Spr	14048914
23.	N.S.	**PAD,** Frt Spr (Part of #26)	
24.	8.915	**NUT,** Hex (M10 x 1.5)	
25.	7.425	**RETAINER,** Frt Spr	14044557
26.	7.412	**SPRING,** Frt (Exc H.D. Susp FE7) (Incl #23, 27, 28, 29)	14045781
	7.412	**SPRING,** Frt (W/H.D. Susp FE7) (Incl # 23, 27, 28, 29)	14045782
27.	N.S.	**CUSHION,** Frt Spr (Part of #26)	
28.	N.S.	**BEARING,** Frt Spr (Part of #26)	
29.	N.S.	**SEAT,** Frt Spr (Part of #26)	
30.	8.900	**BOLT,** Hex Flg Hd (M8 x 1.25 x 25)	
31.	7.425	**SHIM,** Frt Spr Ret	14047771
32.	7.380	**BRACKET,** Frt S/Abs Lwr-LH	14048973
	7.380	**BRACKET,** Frt S/Abs Lwr-RH	14048974
33.	8.900	**BOLT,** Hex Flg Hd (M8 x 1.25 x 35) (10 9)	
34.	7.388	**INSULATOR,** Frt Stab Shf Link Lwr	14048975
35.	8.900	**BOLT,** Hex (M10 x 1.5 x 60)	
36.	7.388	**INSULATOR,** Frt Stab Shf Link Upr	14076149
37.	7.240	**LINK,** Frt Stab Shf	14048977
38.	7.425	**BOLT,** Frt Spr Ret	14076142
39.	7.242	**CLAMP,** Frt Stab Shf Insl	14048971
40.	7.243	**INSULATOR,** Frt Stab Shf	14060134
41.	7.242	**BRACKET,** Frt Stab Shf	14048970
42.	7.241	**SHAFT,** Frt Stab (Exc H.D. Susp FE7)	14060132
	7.241	**SHAFT,** Frt Stab (W/H.D. Susp FE7)	14048992
43.	6.178	**WASHER,** Frt Upr Cont Arm	14047733
44.	6.178	**SHIM,** Frt Upr Cont Arm (.030 Thk)	14047730
45.	8.917	**NUT,** Hex Flanged Prev Torq (M10 x 1.5) (Cad 10)	
46.	N.S.	**REINFORCEMENT,** Frt Shock Tower (*2)	
47.	8.921	**NUT,** Spr "U" Multi-Th (M8 x 1.25)	
48.	8.900	**BOLT,** Hex Flg Hd (M10 x 1.5 x 45)	
49.	6.178	**BOLT,** Frt Upr Cont Arm	14047781
50.	7.003	**FRAME,** Drivetrain & Frt Susp	14044544
51.	7.039	**SHIM,** Drivetrain & Frt Susp Frm (1mm as req'd)	14060122
52.	N.S.	**TOWER,** Frt S/Abs(*2)	
53.	8.917	**NUT,** Hex Prev Torq (M10 x 1.5)	
54.	7.389	**RETAINER,** Frt S/Abs Insl	14044559
55.	7.388	**INSULATOR,** Frt S/Abs Upr	14044560
56.	7.388	**INSULATOR,** Frt S/Abs Lwr	14044561
57.	7.039	**SPACER,** Drivetrain & Frt Susp Frm	14044588
58.	7.345	**ABSORBER,** Frt Shk (Exc H.D. Susp FE7)	4993594
	7.345	**ABSORBER,** Frt Shk (W/H.D. Susp FE7)	4993596
59.	7.347	**NUT,** Frt S/Abs Lwr Mtg	14076121
60.	6.163	**WASHER,** Frt Upr Cont Arm	14060157
61.	8.917	**NUT,** Hex Flg Prev Torq (M12 x 1.75)	
62.	6.508	**GEAR,** Hyd Strg R & P (Compl) (Exc H.D. Susp FE7) (*3)	7840276
	6.508	**GEAR,** Hyd Strg R & P (Compl) (W/H.D. Susp FE7) (*3)	7841041
63.	8.900	**BOLT,** Hex Flg Hd (M8 x 1.25 x 50)	
64.	8.917	**NUT,** Hex Flg Prev Torq (M12 x 1.75)	
65.	6.174	**WASHER,** Frt Lwr Cont Arm (M13 ID 24 OD 3 Thk) (5.383)	14047767
66.	6.170	**BOLT,** Frt Lwr Cont Arm	14049339

NOTE 1: Refer to BRAKE CALIPER illustration in Group 4.000 for detail information.
NOTE 2: Part of MEMBER, Frt, Sides & Cross, Group 7.010.
NOTE 3: Refer to RACK & PINION STEERING illustration in Group 6.000 for detail information.

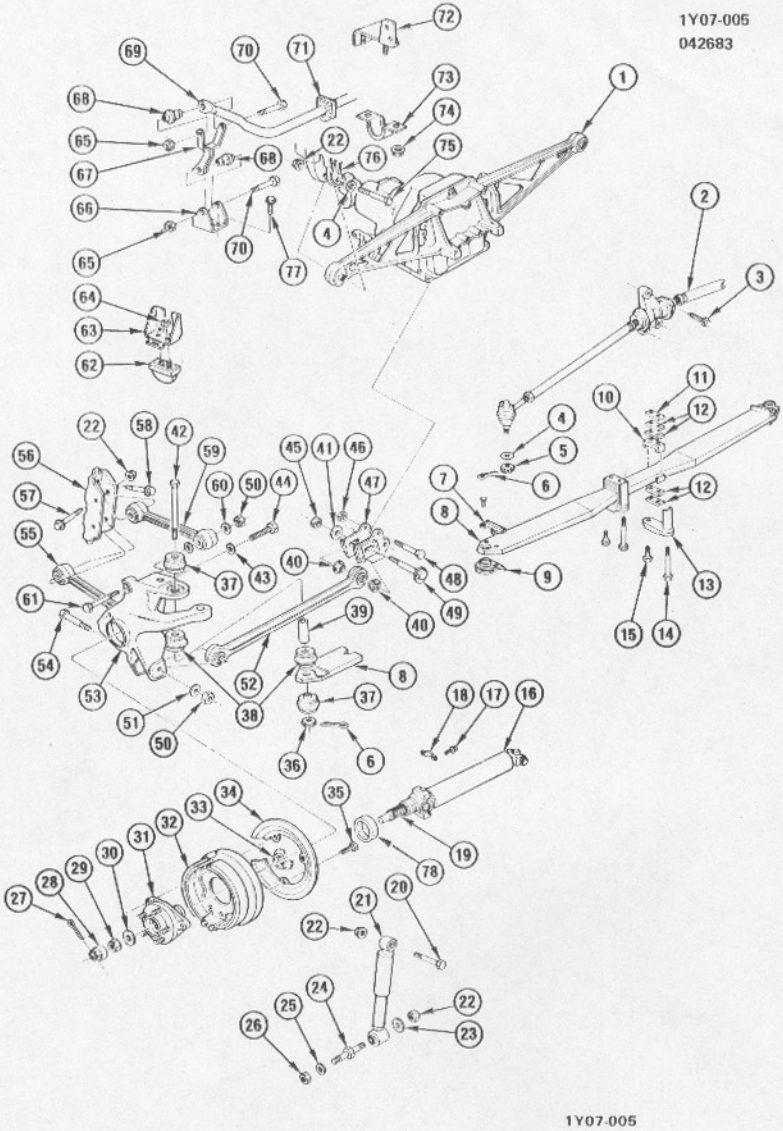


Figure 35 — 1984 "Y" REAR SUSPENSION

1Y07-005

1984 "Y" REAR SUSPENSION

1.	5.505	**CARRIER,** Diff (Comp) (See Catalog) (*1)	
2.	5.415	**ROD,** Rr Axle Tie	14060139
3.	5.418	**BOLT,** Rr Axle Tie (7.425)	14076142
4.	5.418	**WASHER,** Rr Axle Tie Rod (6.164)	14047779
5.	5.418	**NUT,** Rr Axle Tie Rod	14050089
6.	8.938	**PIN,** Cotter (M3.2 x 25)	
7.	N.S.	**RETAINER,** Rr Spr Upr (Part of #8)	
8.	7.503	**SPRING,** Rr (Exc H.D. Susp FE7) (Incl #7, 9, 10)	14045785
	7.503	**SPRING,** Rr (W/H.D. Susp FE7) (Incl #7, 9, 10)	14045786
9.	N.S.	**RETAINER,** Rr Spr Lwr (Part of #8)	
10.	N.S.	**CUSHION,** Rr Spr (Part of #8)	
11.	7.545	**INSULATOR,** Rr Spr	14044575
12.	7.518	**SPACER,** Rr Spr (Exc H.D. Susp FE7)	14048950
	7.518	**SPACER,** Rr Spr (W/H.D. Susp FE7)	14044572
13.	7.518	**PLATE,** Rr Spr Anc	14048993
14.	8.900	**BOLT,** Hex Flgd Hd (M10 x 1.5 x 100)	
15.	8.900	**BOLT,** Hex Flgd Hd (M10 x 1.5 x 30)	
16.	5.420	**SHAFT,** Rr Whl Dr Ujt (W/A.T. Exc 6-Way Seat)	14046912
	5.420	**SHAFT,** Rr Whl Dr Ujt (W/M.T.) (W/A.T. & 6-Way Seat)	14041925
	5.425	**REPAIR KIT,** Ujt, Rr Whl Dr Shf	14067677
17.	5.586	**BOLT,** Rr Whl Dr Ujt Shf Ret	14046904
18.	5.586	**RETAINER,** Rr Whl Dr Ujt Shf	14046907
19.	5.806	**SPINDLE,** Rr Whl	14055941
20.	7.347	**BOLT,** Rr S/Abs Upr (5.418)	14048989
21.	7.345	**ABSORBER,** Rr Shk (Exc H.D. Shk)	4993595
	7.345	**ABSORBER,** Rr Shk (W/H.D. Shk)	4993597
22.	8.917	**NUT,** Hex Flgd (M12 x 1.75)	
23.	7.347	**WASHER,** Rr S/Abs	14047726
24.	7.347	**STUD,** Rr S/Abs	14044582
25.	7.347	**WASHER,** Rr S/Abs (5.383)	10019448
26.	8.917	**NUT,** Hex Prev Torq (M14 x 2)	
27.	8.938	**PIN,** Cotter (M4 x 40)	
28.	5.806	**CAP,** Rr Whl Spdl Nut (6.056)	562248
29.	5.806	**NUT,** Rr Whl Spdl	562247
30.	5.806	**WASHER,** Rr Whl Spdl (6.056)	562249
31.	5.806	**HUB,** Rr Whl	7466924
	5.812	**BOLT,** Rr Whl Mtg	9590476
32.	N.S.	**PLATE ASM,** Rr Park Brk & Clpr Mtg (*2)	
33.	5.806	**WASHER,** Rr Whl Spdl	14076924
34.	5.002	**SHIELD,** Rr Brk-LH	14055965
	5.002	**SHIELD,** Rr Brk-RH	14055966
35.	8.977	**SCREW,** Hex Fl Wa Tap (M6.3 x 1.81 x 16)	
36.	7.530	**NUT,** Rr Spr (6.164)	14048922
37.	7.545	**INSULATOR,** Rr Spr	14044580
38.	7.545	**INSULATOR,** Rr Spr (This location only required W/H.D.Susp FE7)	14044580
39.	7.535	**SPACER,** Rr Spr Insl (W/H.D. Susp FE7 only)	14060140
40.	6.170	**BUSHING,** Rr Whl Spdl Rod	14060166
41.	6.172	**CAM,** Rr Whl Spdl Rod Adj	14060163
42.	7.529	**BOLT,** Rr Spr	14044578
43.	5.383	**WASHER,** Driveline Supt (M13 ID 24 OD 3 Thk)	14047787
44.	5.812	**BOLT,** Rr Whl Hub	14046943
45.	5.415	**NUT,** Rr Whl Spdl Rod Adj	14060164
46.	8.917	**NUT,** Hex Flgd Prev Torq (M10 x 1.5)	
47.	5.380	**BRACKET,** Rr Whl Spdl Rod	14060165
48.	8.900	**BOLT,** Hex Flg Hd (M10 x 1.5 x 75)	
49.	5.806	**BOLT,** Rr Whl Spdl Rod Adj	14060162
50.	8.915	**NUT,** Hex (M16 x 2)	
51.	5.415	**WASHER,** Rr Whl Spdl Rod	14047725
52.	5.415	**ROD,** Rr Whl Spdl-LH	14048947
	5.415	**ROD,** Rr Whl Spdl-RH	14048948
53.	5.380	**KNUCKLE,** Rr Susp-LH	14041901
	5.380	**KNUCKLE,** Rr Susp-RH	14041902
54.	8.900	**BOLT,** Hex (M16 x 2 x 110)	
55.	5.415	**ROD,** Rr Whl Spdl Lwr Cont	14048931
56.	5.418	**BRACKET,** Rr Whl Spdl Cont Rod	14071742
57.	5.418	**BOLT,** Rr Whl Spdl Rod	14048989
58.	8.900	**BOLT,** Hex Flgd Hd (M12 x 1.75 x 50)	
59.	5.415	**ROD,** Rr Whl Spdl Upr Cont	14048930
60.	5.415	**WASHER,** Rr Whl Spdl Rod to Knu	14044574
61.	5.418	**BOLT,** Rr Whl Spdl Rod to Knu	14048990
62.	7.535	**BUMPER,** Rr Susp Jounce	14044577
63.	7.535	**BRACKET,** Rr Susp Jounce Bpr	14064511
64.	8.915	**NUT,** Hex (M10 x 1.5)	
65.	7.245	**NUT,** Rr Stab Shf Link	14060131
66.	7.242	**BRACKET,** Rr Stab Shf	14060115
67.	7.240	**LINK,** Rr Stab Shf	14060113
68.	7.243	**INSULATOR,** Rr Stab Shf Link	14060114
69.	7.241	**SHAFT,** Rr Stab (Exc H.D. Susp FE7)	14060109
	7.241	**SHAFT,** Rr Stab (W/H.D. Susp FE7)	14060110
70.	7.240	**BOLT,** Rr Stab Shf Link	14060130
71.	7.243	**INSULATOR,** Rr Stab Shf (Exc H.D. Susp FE7)	14060111
	7.243	**INSULATOR,** Rr Stab Shf (W/H.D. Susp FE7)	14060112
72.	7.242	**SUPPORT,** Rr Stab Shf Brkt-LH	14064521
	7.242	**SUPPORT,** Rr Stab Shf Brkt-RH	14064522
73.	7.242	**BRACKET,** Rr Stab Shf	3913863
74.	8.915	**NUT,** Hex Flg (M8 x 1.25)	
75.	5.506	**BOLT,** Diff Carr	14060116
76.	N.S.	**BRACKET,** Diff Carr (Part of MEMBER, Rr Si Group 7.006)	
77.	8.900	**BOLT,** Hex (M8 x 1.25 x 30)	
78.	5.822	**SEAL,** Rr Whl Hub Brg	14046941

NOTE 1: CARRIER DIFFERENTIAL details shown in Group 5.000 illustration.
NOTE 2: Refer to PARK BRAKE illustration in Group 4.000 for details.

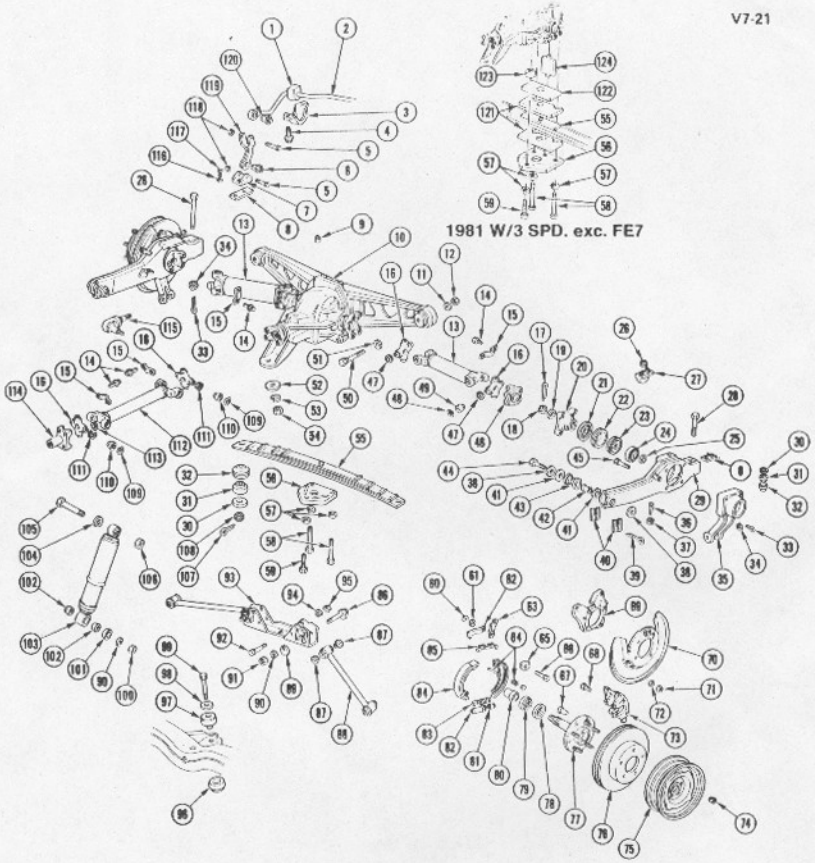


Figure 36 — 1980-82 "Y" REAR SUSPENSION

V7-21

1980-82 "Y" REAR SUSPENSION

1.	**BUSHING,** Rr Stab Shaft	7.243
2.	**SHAFT,** Rr Stab	7.241
3.	**BRACKET,** Rr Stab Shaft	7.242
4.	**SCREW,** Rr Stab Shaft Brkt	7.242
5.	**BOLT** (3/8"-16 x 1 3/8")	8.900
6.	**BUSHING,** Rr Stab Shaft Link	7.243
7.	**BRACKET,** Rr Stab Shaft Link	7.242
8.	**PLATE,** Rr Stab Shaft Link	7.245
9.	**PLUG,** W/Instruction Tag (1 7/64" OL)	5.400
10.	**CARRIER,** Diff W/Cap	5.505
11.	**WASHER,** Lock	8.931
12.	**NUT,** Hex	8.900
13.	**SHAFT,** Axle	5.420
14.	**BOLT,** Hex (7/16"-20 x 1 1/8")	8.900
15.	**RETAINER,** Prop & Axle Shaft U-Joint (5.428)	5.380
16.	**REPAIR KIT,** U-Joint	5.548
17.	**PIN,** Rr Whl Spindle	8.938
18.	**NUT,** Hex Slot (3/4"-20)	5.829
19.	**WASHER** (7/32")	5.830
20.	**FLANGE,** Rr Whl Spindle	5.806
21.	**DEFLECTOR,** Rr Whl Spindle	5.425
22.	**SHIELD,** Rr Whl Inr Bearing	5.815
23.	**SEAL,** Rr Inr Bearing	5.822
24.	**BEARING,** Rr Whl Inr	5.855
25.	**SHIM,** Rr Wheel	5.855
26.	**NUT,** Hex (3/8"-16)	8.917
27.	**BUMPER,** Rr Spring	7.535
28.	**BOLT,** Rr Spring	7.529
29.	**ARM,** Torque Control	5.382
30.	**RETAINER,** Spring Otr	7.545
31.	**CUSHION,** Rr Spring	7.545
32.	**RETAINER,** Spring Inr	7.545
33.	**PIN,** Cotter	8.938
34.	**NUT,** Hex Slot (5/8"-18)	8.917
35.	**SUPPORT,** Control Arm to Spindle	5.380
36.	**PIN,** Cotter	8.938
37.	**NUT,** Diff Carrier Mount (Hex Slot; 7/16"-20)	8.000
38.	**WASHER,** Torque Control Arm	5.383
39.	**PIN,** Cotter	8.938
40.	**SHIM,** Rr Axle Torque Arm	5.380
41.	**PLATE,** Rr Susp Torque	5.381
42.	**RETAINER,** Torque Arm Bushing	5.381
43.	**BUSHING,** Upr Rr	5.381
44.	**BOLT,** Torque Control Arm	5.383
45.	**BOLT,** Hex (3/8"-24 x 2 7/8")	5.002
46.	**FLANGE,** Rr Whl U-Joint	5.425
47.	**SEAL,** Yoke Bearing (Also Part of #16)	5.430
48.	**LOCK RING,** U-Joint (Also Part of #16)	5.586
49.	**BEARING,** U-Joint (Also Part of 16)	5.428
50.	**BOLT,** Hex	8.900
51.	**WASHER**	8.929
52.	**PLATE,** Body Mtg Cushion	N.S.
53.	**WASHER,** Lock (7/16")	8.931
54.	**NUT,** Hex (7/16"-14)	8.915
55.	**SPRING,** Rear	7.503
56.	**PLATE,** Rr Spring	7.518
57.	**WASHER,** Lock (7/16")	8.931
58.	**BOLT,** Hex (M12 x 1.75 x 90)(10.9 PC)	8.900
59.	**BOLT,** Hex (M12 x 1.75 x 30)	8.900
60.	**CLIP,** Parking Lever	5.056
61.	**WASHER,** Pivot pin	5.158
62.	**STRUT,** Parking Brake Shoe Lever	5.150
63.	**LEVER,** Parking Brake Shoe	5.149
64.	**PIN KIT,** Brake Shoe	5.043
65.	**PLATE,** Brake Shoe Guide	5.045
66.	**BOLT,** Hex (7/16"-20 x 1 1/16")	5.056
67.	**RIVET,** Spindle	N.S.
68.	**BOLT,** Hex (7/16"-20 x 1 1/8")	8.900
69.	**BRACKET,** Brake Flange Rr	5.001
70.	**PLATE,** Parking Brake	5.001
71.	**NUT,** Hex (3/8"-24)	8.915
72.	**WASHER** (3/8")	8.931
73.	**HOUSING ASM,** Brake Caliper	4.665
74.	**NUT,** Whl (7/16"-20	5.813
75.	**WHEEL ASM**	5.803
76.	**DISC,** Rr Whl Brake	5.809
77.	**SPINDLE,** Rr Whl	5.806
78.	**SEAL,** Rr Whl Otr Brg	5.822
79.	**BEARING ASM,** Rr Whl Otr	5.855
80.	**SPACER,** Bearing	5.816
81.	**SOCKET,** Adj Screw	5.111
82.	**SCREW,** W/Nut, Parking Brake Adj	5.110
83.	**SPRING,** Parking Brake	5.026
84.	**SHOE KIT,** Parking Brake	5.017
85.	**SPRING,** Parking Brake	5.026
86.	**CAM,** Spindle Support Strut	5.417
87.	**CAP,** Rr Whl Strut Rod Bushing	5.417
88.	**ROD,** Rr Whl Strut(* 1)	5.415
89.	**CAM,** Rr Whl Strut	5.417
90.	**WASHER,** Lock (1/2")	8.931
91.	**NUT,** Hex (1/2"-20)	8.921
92.	**BOLT,** Hex (M10 x 1.5 x 70)	8.900
93.	**BRACKET,** Rr Whl Sturt	5.380
94.	**WASHER,** Lock	8.931
95.	**NUT,** Hex (M10 x 1.5 x 70)	8.915
96.	**INSULATOR,** Diff Carrier Mt Lwr	9.023
97.	**INSULATOR,** Diff Carrier Mt Upr	9.023
98.	**WASHER** (11/32 x 1 1/8 x 1/8)	8.929
99.	**BOLT,** Hex (7/16"-14 x 3 1/8")	8.900
100.	**NUT,** Shock Abs Lwr (1/2"-20)	5.383
101.	**RETAINER,** Lwr Grommet Otr	7.389
102.	**GROMMET,** Shock Abs Lwr	7.388
103.	**ABSORBER,** Rr Shock	7.345
104.	**WASHER,** Lock	8.931
105.	**BOLT,** Shock Abs Upr	7.374
106.	**NUT** (7/16"-20)	8.916
107.	**PIN,** Cotter	8.938
108.	**NUT,** Hex Slot (9/16"-18)	8.917
109.	**LOCK RING,** U-Joint (Also Part of #16)	5.586
110.	**BEARING,** U-Joint (Part of #16)	5.548
111.	**SEAL,** U-Joint (Also Part of #16)	5.560
112.	**SHAFT,** Propeller	5.544
113.	**SPACER,** Trunnion Bearing	5.566
114.	**YOKE,** U-Joint Sleeve Frt	5.555
115.	**SHAFT,** Rr Whl Spindle Rod	5.381
116.	**WASHER,** Lock	8.931
117.	**BOLT** (5/16"-18 x 3/4")	8.900
118.	**NUT,** Hex (3/8"-16)	8.915
119.	**LINK,** Rr Stab Shaft	7.240
120.	**BUSHING,** Rr Stab Shaft Link	7.243
121.	**SPACER,** Rr Spr	7.518
122.	**INSULATOR,** Rr Spr	7.545
123.	**SPACER,** Rr Spr (1/2" ID 11/16" OD 3/16" Thk)	7.518
124.	**SPACER,** Rr Spr, Plate to Diff	7.518